

Our ref: **25.1.201/EKi/6682**

11 September 2010

tie limited
CityPoint
65 Haymarket Terrace
Edinburgh
EH12 5HD

**Bilfinger Berger–Siemens– CAF
Consortium**

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For the attention of Steven Bell – Tram Project Director

Dear Sirs,

**Edinburgh Tram Network Infraco
Project Carlisle – Revised Infraco Full and Final Proposal**

We refer to our letter dated 29th July 2010 ref. 25.1.201/EKi/6338 and your letter dated 7th September 2010 ref. INF CORR 5990.

Following the lengthy discussions between tie and Infraco representatives regarding the initiative known as Project Carlisle (a Guaranteed Maximum Price Proposal based on a reduced scope and accelerated programme in an effort to allow tie to obtain a cost effective and revenue generating tram network) we herewith enclose our revised full and final proposal.

We have incorporated, where possible, the requirements of tie although we are fully cognisant that not every tie desire has been incorporated, and as a matter of clarity not every Infraco cost has been included.

You will be aware of a number of promises and commitments made by your representative to resolve the considerable number of outstanding issues in respect of Third Party Approvals to enable us to reduce our exclusions/caveats. Unfortunately not a single one of these commitments has been met and thus our exclusions/caveats must remain.

As we previously offered, we remain willing to discuss how our Full and Final Offer has been established with the Gordon Harris Partnership. We are not however, prepared to entertain any further discussions around the tie 'Counter Proposal' received by us on 8th September 2010 as we consider tie's "Counter Proposal" to be wholly and totally unrealistic both in terms of it's pricing structure and level of risk transfer back to Infraco.

In conjunction with Gordon Harris Partnership and our civil works Subcontractors, we have now been able to fully review and reduce our previous submission in an effort to reach an agreement on Project Carlisle. We are confident that these Sub-Contractors have given all that they possibly can.

Due to the protracted negotiations in respect of Project Carlisle, key construction activities on the critical path have slipped resulting in an overall slippage of the completion date by some 5 (five) weeks.

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The Guaranteed Maximum Price has therefore been developed in the light of the above as described in the attached proposal and pertains to the attached scope and programme. For the avoidance of doubt, this Proposal relates solely to the scope and corresponding programme of the Project Carlisle Initiative and is wholly without prejudice to our rights and remedies under the Contract until such time as it is incorporated by a tie Change Order to that Contract, upon which time it shall supersede all estimates and proposals in relation to the Edinburgh tram Network.

No party may rely on any statement, declaration or representation contained herein whether express or implied, in any court proceedings, arbitration, adjudication, mediation or any other form of determination or negotiation without the express written permission of the Infracore. This Proposal shall not constitute an offer or create any legally binding obligation on the Infracore.

As always, we will avail ourselves to meet at your convenience to discuss this Revised Proposal.

Yours faithfully,


M Foerder
Project Director
Bilfinger Berger Siemens CAF Consortium



Encl: Revised 'Project Carlisle' Proposal

cc: R. Walker – Bilfinger Berger
M. Flynn – Siemens
M. Berrozpe – Siemens
A. Brandenburger - Siemens
A. Campos - CAF
A. Urriza - CAF

INFRACO "PROJECT CARLISLE" PROPOSAL

Revised 12 September 2010

**INFRACO CONSIDERS THIS PROPOSAL TO BE
CONFIDENTIAL INFORMATION AND SUBMITS IT
WITHOUT PREJUDICE**

**INFRACO RESERVES THE RIGHT TO REVISE,
RESUBMIT, AND/OR WITHDRAW THIS PROPOSAL
AT ITS CONVENIENCE**

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PREFACE

This Revised Infraco "Project Carlisle" Proposal takes into account its predecessor, dated 29 July 2010, and tie's letter dated 7 September 2010 (INF CORR 5990). tie's letter of the 7 September 2010 and accompanying documents fall considerably short of our threshold for acceptance. The overarching theme of the document presents a picture of the contract tie wishes they had executed with Infraco, and from a perspective that Infraco have no rights. As you are well aware, Infraco enjoys the express and implied rights and remedies of the executed Infraco Contract.

We have no intention to further use this document to convey our disappointment or to correct the many inaccurate statements in your letter. Based upon the report we have received from our Mr. Kitzman, regarding his discussions with your Messrs. Rush and Molyneux, it seems that several issues still remain to be reconciled between the Parties, specifically the scope, programme, and pricing.

In an attempt to pursue a reasonable solution to the Project, we have reassessed these items and have updated our Proposal accordingly.

With regard to the revised scope, as discussed, this Revised Proposal does not include the works east of Haymarket.

To clarify, it is tie who do not want Infraco to undertake the works east of Haymarket. In the event that tie would require the E&M installation works (train control, traffic control, SCADA, Communication, electrification and Over Head Line systems) to be completed east of Haymarket, Siemens would be willing to enter into discussions with tie and provide a quotation to tie in response to a clear scope of work, programme, and a standard set of terms and conditions issued by tie, in which Siemens would be a subcontractor to tie.

The transfer of substantial risks, as outlined in your letter, many of which are not quantifiable, cannot be accepted without Infraco substantially increasing its cost. We have taken on the risk for items that we believe are quantifiable, and have noted as much later in this document. tie must retain the risk for the other items simply due to the fact that tie have full control to avoid the additional costs. To clarify, the majority of the remaining risk issues are associated with various items that have continued to remain in flux due to tie, CEC, or other third parties, in spite of numerous commitments to resolve these matters. Tie and CEC could simply avoid all of these additional costs by committing contractually to the GMP Drawings previously provided by Infraco.

In respect of programme, we have reviewed the Sectional Completion Dates identified in your letter, however we do not believe they represent a realistic reflection of the works yet to complete. The Programme, included in Appendix 5, has been exclusively developed for the implementation of Project Carlisle works. This programme is challenging but achievable and Infraco is fully committed to delivering it. The estimated price to complete the work from Airport to Haymarket is roughly £405.5Mio plus €6Mio, as detailed in the attached price build up. We have no interest in further discussing and debating the price. This Revised Proposal is Infraco's Full and Final Offer to complete the works defined herein.

For the avoidance of doubt this Revised Proposal relates solely to the scope and corresponding programme of the Project Carlisle initiative and to that extent shall supersede all estimates and proposals provided in relation with the Edinburgh Tram Network (ETN) once implemented by a tie Change Order acceptable to Infraco. This proposal is submitted without prejudice to Infraco's whole rights and remedies and no party may seek to rely on any statement, declaration

or representation contained within this proposal, whether express or implied, in any court proceedings, arbitration, adjudication, mediation or other form of determination or negotiation without the express written content of Infraco. This proposal shall not constitute an offer or create any legally binding obligation on Infraco.

EXECUTIVE SUMMARY

Introduction

Further to your request, this Revised Proposal is Infraco's Full and Final Offer to complete the works defined herein.

The GMP Scope

We have revised our Proposal to reflect the scope Infraco will be able to agree on. In essence, we will carry out the works shown on the GMP Drawings for scope of work between the Airport and Haymarket, and the enabling works.

Our Pricing Assumptions

As a result of the above, we are able to offer our Guaranteed Maximum Price on the basis of a minimal number of Pricing Assumptions. We would like to take this opportunity to assure you that we have looked to minimise the number of Pricing Assumptions to provide greater certainty to both Parties and for the benefit of the Project as a whole.

The main items are summarised as follows:

1. Infraco will build to the GMP Drawings subject to the qualifications set out in Appendix 4 of this Revised Proposal. The GMP Drawings will not be amended and Infraco will not be required to construct anything other than the GMP Drawings other than by way of a tie Change
2. tie will lend all available assistance to ensure CEC will approve the GMP Drawings in expedient fashion and in accordance with the Programme.
3. Infraco will assume the risk of dealing with known quantities of contaminated ground (including by invasive species), as outlined later.
4. Infraco will not take any risk associated with the discovery and/or replacement of utilities
5. Infraco has accepted the risk to remove and replace soft materials to the extent identified on the GMP Drawings
6. Infraco will not take the risk on obstructions or voids being discovered below the original ground level
7. Infraco and tie shall agree a revised Milestone Payment Schedule acceptable to both Parties,
8. CEC will grant all consents and technical approvals for the Integrated Trackform design shown on the GMP Drawings

The GMP Programme

Our proposed Programme is set out at Appendix 5. This shows Sectional Completion Dates of:

1. Section A – 9 June 2011
2. Section B – 20 December 2011
3. Section C – End of construction 21 June 2012 - End of Testing & Commissioning 19 September 2012
4. Section D – 18 December 2012

The Programme which was exclusively set up for the implementation of Project Carlisle is challenging but achievable and Infracore is fully committed to delivering it.

The Guaranteed Maximum Price

In light of the above, we are able to offer a 'Guaranteed Maximum Price' of £405,531,217 and €5,829,805. We believe this is a fair and reasonable offer which we hope, when seen as part of this Revised Proposal, will prove acceptable to tie.

A detailed breakdown is provided at Appendix 1.

Revised Change Mechanism

We believe that Project Carlisle offers an excellent opportunity to remedy what has previously been a major source of contention on the Project - namely Clause 80. Our proposal will commit Infracore to working whilst Estimates are agreed subject to payment being received for works carried out during that period.

The revised mechanism would operate as follows:

1. tie will be required to issue a tie Notice of Change upon the occurrence of a change event, or upon Infracore notifying it of such an occurrence.
2. Failure to issue a tie Notice of Change will result in the matter being referred to DRP.
3. Infracore to prepare an Estimate within existing contractual timescales, following receipt of a tie Notice of Change.
4. A finite period to be allowed for agreement of the Estimate failing which the matter will be referred to DRP.
5. Infracore will commit to carrying out the works associated with the tie Change in advance of agreement or determination of the Estimate, and where necessary, in advance of the production of an Estimate in accordance with the Programme.
6. Infracore to be paid, on an interim basis in line with the Estimate.
7. Note that as set out above, Infracore will continue to build the design set out in the GMP Drawings until it receives a tie Notice of Change instructing otherwise.

It is our firm belief that the above changes offer an equitable solution for both Parties, allowing uninterrupted progress of the Works to continue and a swifter delivery of the ETN.

Priority of Project Carlisle

Given the innovative nature of Project Carlisle and the impact of our Revised Proposal on the existing Infraco Contract, both Parties would be required to accept that the product of this Revised Proposal will take precedence over any conflicting existing Infraco Contract obligation.

Conclusion

We believe that our proposal provides an excellent opportunity for both sides to move forward in a positive direction. The key aspects of our proposal can be summarised as follows:

1. A Guaranteed Maximum Price based on a well-developed design
2. A fully optimised Programme providing the most sensible and expedient method of delivering the Project
3. A significantly reduced number of Pricing Assumptions
4. A simplified Change mechanism which allows progress to continue while Clause 80 runs its course.

Whilst we anticipate that much time and work will be required to align the Infraco Contract with the changes brought about by Project Carlisle, not least to the Employer's Requirements and Infraco Proposals, there is an immediate need to resolve outstanding issues relating to the approval of form to Sub Contract for key Sub-Contractors and the Permits to Commence work in order to demonstrate the benefit of Project Carlisle to the general public.

We also believe that the success of the Project could be ensured by the appointment of a mutually acceptable full time mediator by tie to resolve differences, and continually engage both Parties in constructive dialogue.

NOTE: In an effort to simplify your review of this Revised Proposal we have maintained the previous Sectioning.

DEFINITIONS USED IN THIS PROPOSAL

"Project Carlisle Change Order" for the purposes of this Revised Proposal, refers to the issue by tie of a Change Order to incorporate the 'Project Carlisle' initiative and associated Minute of Variation.

"GMP Drawings" means the GMP Drawings, Specifications and Schedules, listed in Appendix 3 of this Revised Proposal.

"Guaranteed Maximum Price" means, on the basis of the scope and assumptions referenced in this Revised Proposal, the sum set out in Appendix 1 as the same may be varied in accordance with the principles set out in this Revised Proposal and the remaining terms of the Infraco Contract.

"Actual Cost" means all direct and indirect cost/expenditure incurred whether on or off Site including overheads, finance, and other charges and allowances allocable thereto.

"Estimated Actual Cost" means, a reasonable estimate of Actual Costs, based upon the available information, terms, and conditions known at the time of estimation. It is anticipated that Estimated Costs may vary from the Actual Costs.

"Programme" means the Programmes contained in Appendix 5

APPENDIX 1

GUARANTEED MAXIMUM PRICE

The Guaranteed Maximum Price is comprised of the following Infraco Prices (further detailed in Appendix 1.1 – 1.4 respectively), exclusive of Value Added Tax and in Pounds Sterling and Euro:

Bilfinger Berger Civil UK Ltd.	£215,300,646
Siemens	£118,601,221
CAF	£55,781,634
and	€5,829,805
SDS	£15,847,716
Total	£405,531,217
and	€5,829,805

APPENDIX 1.1

Bilfinger Berger UK Ltd - Guaranteed Maximum Price Breakdown

Appendix 1.1 - BB – GMP Breakdown

For clarity, the BB GMP Price now includes for previous GMP Exclusions / Qualifications for the following items (as detailed in Appendix 4):

- Known quantities of contaminated ground,
- Protection of Services,
- New Ingleston Limited Future Proofing,
- CEC Planning Requirements at Roseburn Viaduct.

Bilfinger Berger Civil UK Limited : GMP Summary

10/09/2010

1 . Direct Cost

Item	Description	Original SubContract Order	Prelims and Method Related Charges			Construction Works				Risk / Opportunity	Grand Total	
			to 30 June 2010	to Completion	(6)=(4)+(5)	Original Subcontract Work	Change		Construction Works Total			
(1)	(2)	(3)	(4)	(5)	(6)=(4)+(5)	(7)	(8)	(9)	(10)	(11) = (7 to 10)	(12)	(13) = (3)+(11)+(12)
Total for Direct Cost		67,320,859	9,930,764	18,287,295	28,218,059	50,483,975	663,527	30,012,384	27,996,323	109,156,209	2,611,433	139,985,701

* At the 30th June 2010

Previous GMP Exclusions / Qualifications now priced	8,058,450	8,058,450
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2 . SDS Provider

Item	Description	Original Order	Scope Works			Total (excl Risk & Opportunity)	Risk / Opportunity	Grand Total	
			Original Work	Change					
(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)=(4 to 7)	(9)	(10)=(8)+(9)
Total for SDS Provider		4,983,815	4,983,815	4,806,924	1,358,918	4,698,059	15,847,716	-	15,847,716

* At the 30th June 2010

3. In Direct Cost

Item	Description	Indirect Cost	Risk / Opportunity	Grand Total
(1)	(2)	(3)	(4)	(5)=(3)+(4)
Total for In Direct Cost		41,203,008	5,040,000	46,243,008

4. Summary of Direct, SDS and Indirect Costs

4. Summary of Direct, SDS and Indirect Costs		210,134,874.76
Overheads	7%	14,709,441.23
Profit	3%	6,304,046.24
GMP Grand Total		231,148,362.23

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GRAND TOTAL											
Item	Description	Original SubContract Order	Prelims and Method Related Charges		Construction Works				Risk / Opportunity	Grand Total	
			to 30 June 2010	to Completion	Original Subcontract Work	Change		Construction Works Total			
(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(6 to 9)+(10)	(11)	
						Already Submitted* and Agreed	Already Submitted* and Not Agreed	New Submission			
1A	McKean & Co	2,620,066	671,292	1,333,140	1,628,076	879,730	42,003	2,598,759	5,148,568	447,000	7,600,000
1B	Crummock (Scotland) Ltd		184,430	12,500		392,622		5,313	397,935		594,865
1C	Not Required										
1C/1D	MacKenzie Construction Ltd	1,977,106			1,977,106		7,707,463	228,828	9,913,397		9,913,397
	Crummock (Scotland) Ltd			2,502,544		4,917	2,507,461		2,507,461		
	Class One			345,950		29,298	375,248		375,248		
	Others			69,796			69,796		69,796		
	Totals	1,977,106			1,977,106	10,625,753	263,043	12,865,902		12,865,902	
	Other Work in 1C/1D (non PSSA)	447,095			447,095		1,668,717		2,115,812		2,115,812
	1C/1D Totals	2,424,201			2,424,201		12,294,470	263,043	14,981,714		14,981,714
1D	McKenzie Construction Ltd	3,871,941	40,144	174,856	3,355,254	(2,865,126)	(490,128)		(0)		215,000
On	Other On Street Works	3,327,111	1,295,098	459,759	1,618,345	(1,252,666)	77,053		442,732	47,350	2,244,939
ON Street Summary		12,243,319	2,190,964	1,980,255	9,025,876	(2,845,440)	11,923,398	2,867,115	20,970,948	494,350	25,636,518
2A	John Graham (Dromore) Ltd	15,853,507	1,678,000	7,393,252	9,683,666	85,790	1,366,830	3,092,707	14,228,993	225,000	23,525,245
5	Expanded Ltd	12,207,596	2,462,979	2,858,254	9,198,688	73,794	12,284,125	1,003,128	22,559,735	1,466,383	29,347,351
6	Barr Limited	12,588,218	1,643,983	830,413	10,451,039	1,232,033	1,006,610	720,220	13,409,902	305,700	16,189,998
7A	Farrans Ltd	5,583,816	1,198,653	1,897,641	4,291,048	2,117,351	3,431,421	4,191,386	14,031,206	120,000	17,247,500
Off	Other Off Street Works	8,844,403	756,184	3,327,480	7,833,658			16,121,767	23,955,425		28,039,089
OFF Street Summary		55,077,540	7,739,799	16,307,040	41,458,099	3,508,968	18,088,986	25,129,208	88,185,261	2,117,083	114,349,183
OVERALL SUMMARY		67,320,859	9,930,764	18,287,295	50,483,975	663,527	30,012,384	27,996,323	109,156,209	2,611,433	139,985,701

* At the 30th June 2010

PROGRAMME INFORMATION										
Item	Description	Subcontract Order Start Date	Subcontract Contract Period (wks)	Subcontractor Mobilised and On Site	Actual Start Date on Site	Programme Status at 30/6/10	Subcontract Contract Period Revised (wks)	Additional Duration (wks)	Carlisle Programme Subcontract Completion	Weeks from 01/07/10
1A	McKean & Co	19-Jun-09	96	23-Nov-09	09-Mar-10	9 wks BH	104	8	20-Jun-11	50
1B	Crummock (Scotland) Ltd	06-Oct-08	n/a	01-Oct-08	13-Oct-08	n/a	n/a	n/a	n/a	n/a
1C	Not Required	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
1D	McKenzie Construction Ltd	05-Jan-09	88	05-Jan-09	05-Jan-09	n/a	n/a	n/a	n/a	n/a
2A	John Graham (Dromore) Ltd	01-Feb-10	65	23-Nov-09	23-Nov-09	21 wks BH	111	46	21-Mar-12	89
5	Expanded Ltd	27-Oct-08	85	27-Oct-08	27-Oct-08		157	72	03-Nov-11	69
6	Barr Limited	03-Nov-08	68	02-Feb-09	17-Aug-09	On Prog	110	42	16-Dec-10	24
7A	Farrans Ltd	08-Sep-08	56	11-Sep-08	11-Sep-08		152	96	11-Aug-11	57

Edinburgh Tram Network
Project Carlisle : "Guaranteed Maximum Price"
Subcontractor GMP Summary
Subcontractor : McKean & Co (Glasgow) Ltd
Section 1A : Enabling Works

					GRAND TOTAL								
Item	Description	Original SubContract Order	De-Scope for Project Carlisle	Balance of Original SubContract Order	Prelims and Method Related Charges		Construction Works				Risk / Opportunity	Grand Total	
					to 30 June 2010	to Completion	Original Subcontract Work	Change		New Submission			Construction Works Total
(3)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(8 to 11)+(12)	(13)	(6)+(7)-(12)+(13)
1	Preliminaries & General Items	1,679,780	687,790	991,990	671,292	1,333,140						567,000	2,571,432
2	Method Related Charges	18,800	18,800										
3	Track and Formation	1,063,265	1,014,910	48,355			48,355				48,355		48,355
4	Tramstops	448,537	448,537										
5	Accommodation Works	2,050	200	1,850			1,850	8,300	59,224		69,374		69,374
6	Highways	841,184	841,184					1,866	1,200,509		1,202,375		1,202,375
7	Buildings - Substation	26,276	26,276										
8	Structures	1,777,120	199,249	1,577,872			1,577,872	879,730	25,921	320,739	2,804,262		2,804,262
9	OHLE	88,214	88,214					5,916			5,916		5,916
10	Lindsay Road Realignment									953,286	953,286		953,286
11													
12													
13	Risk Review Transfer / Deletion										65,000	(120,000)	(55,000)
14													
	Total	5,945,227	3,325,161	2,620,066	671,292	1,333,140	1,628,076	879,730	42,003	2,598,759	5,148,568	447,000	7,600,000

* At the 30th June 2010

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Section 1B: Leith Walk

GRAND TOTAL											
Item	Description	Original SubContract Order	Prelims and Method Related Charges		Construction Works				Risk / Opportunity	Grand Total	
			to 30 June 2010	to Completion	Original Subcontract Work	Change		Construction Works Total			
(1)	(2)	(3)	(4)	(5)	(6)	Already Submitted* and Agreed	Already Submitted* and Not Agreed	New Submission	(6 to 9)=(10)	(11)	(4)+(5)+(10)+(11)
1	Preliminaries & General Items		184,430	12,500					-		196,930
2	Method Related Charges								-		-
3	Track and Formation					392,622		5,313	397,935		397,935
4									-		-
5									-		-
6									-		-
7									-		-
8									-		-
9									-		-
10									-		-
11									-		-
12									-		-
	Total	-	184,430	12,500	-	392,622	-	5,313	397,935	-	594,865

* At the 30th June 2010

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Edinburgh Tram Network
 Project Carlisle : "Guaranteed Maximum Price"
 Subcontractor GMP Summary
 Subcontractor : MacKenzie Construction

Section 1D reduced to West End of Princes St

GRAND TOTAL											
Item	Description	Original SubContract Order	Prelims and Method Related Charges		Construction Works				Construction Works Total	Risk / Opportunity	Grand Total
			to 30 June 2010	to Completion	Original Subcontract Work	Change		New Submission			
						Already Submitted* and Agreed	Already Submitted* and Not Agreed				
(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(6 to 9)=(10)	(11)	(4)+(5)+(10)+(11)
1	Preliminaries & General Items	516,687	40,144	174,856	-	-	-	-	-	-	215,000
2	Method Related Charges	-	-	-	-	-	-	-	-	-	-
3	Accommodation Works	-	-	-	-	-	-	-	-	-	-
4	Highways	1,584,666	-	-	1,584,666	(1,464,329)	-	-	120,337	-	120,337
5	Landscaping	-	-	-	-	-	-	-	-	-	-
6	Overhead Electrics	110,047	-	-	110,047	(96,642)	-	-	13,405	-	13,405
7	Sub-stations	-	-	-	-	-	-	-	-	-	-
8	Trackwork	1,054,934	-	-	1,054,934	(742,455)	-	-	312,479	-	312,479
9	Tramstops	158,803	-	-	158,803	(158,803)	-	-	-	-	-
10	Drainage	156,500	-	-	156,500	(112,593)	-	-	43,907	-	43,907
11	Traffic Signal Ducting	111,068	-	-	111,068	(111,068)	-	-	-	-	-
12	Street Lighting Ducting (less original bill items)	104,572	-	-	104,572	(104,572)	-	-	-	-	-
13	Tactile Paving at Crossings	22,833	-	-	22,833	(22,833)	-	-	-	-	-
14	Bus Stop Shelters	25,421	-	-	25,421	(25,421)	-	-	-	-	-
15	CBR Testing on Trackbox	26,410	-	-	26,410	(26,410)	-	-	-	-	-
16	Deduct - 380m completed under PSSA	-	-	-	-	-	(490,128)	-	(490,128)	-	(490,128)
17	Risk Review Transfer / Deletion	-	-	-	-	-	-	-	-	-	-
	Total	3,871,941	40,144	174,856	3,355,254	2,865,126	490,128	-	0	-	215,000

* At the 30th June 2010

Other On Street Works

GRAND TOTAL											
Item	Description	Original SubContract Order	Prelims and Method Related Charges		Construction Works				Risk / Opportunity	Grand Total	
			to 30 June 2010	to Completion	Original Subcontract Work	Change		Construction Works Total			
(1)	(2)	(3)	(4)	(5)	(6)	Already Submitted* and Agreed	Already Submitted* and Not Agreed	New Submission	(6 to 9)=(10)	(11)	(4)+(5)+(10)+(11)
1	RD Anderson					1,441			1,441		1,441
2	Lothian and Borders Police					2,845			2,845		2,845
3	Hunter Demolotion					14,500			14,500		14,500
4	Raeburn					368			368		368
5	Harper Generators					330			330		330
6	Daver Steels (Bar & Cable Syst					2,412			2,412		2,412
7	Scottish Water					755			755		755
8	Harper Generators					1,800			1,800		1,800
9	Gallagher Groundworks Ltd/					1,110			1,110		1,110
10	Instalcom Ltd					4,550			4,550		4,550
11	Stanger Testing Services Limit					1,510			1,510		1,510
12	Beattie Chalmers Limited/					8,854			8,854		8,854
13	Traffic Signs under estimate Section 1A						16,067		16,067		16,067
14	Traffic Signs under estimate Section 1C3/1D						60,986		60,986		60,986
15	On Street TM	1,646,163	1,206,410	410,429					-	41,600	1,658,439
16	Substation Housing Design (Portastor)	37,337	37,337		74,517				74,517		111,854
17	Tram Stop Shelter and Ancilliary Equipment Design (RWS)	51,351	51,351	897					-		52,248
18	Street Lighting	1,592,260	-	48,433	1,543,828	(1,293,141)			250,687	5,750	304,870
	Total	3,327,111	1,295,098	459,759	1,618,345	(1,252,666)	77,053	-	442,732	47,350	2,244,939

* At the 30th June 2010

Edinburgh Tram Network
 Project Carlisle : "Guaranteed Maximum Price"
 Subcontractor GMP Summary
 Subcontractor : John Graham (Dromore) Ltd

Section 2A and 5A (part)

			GRAND TOTAL							Grand Total	
Item	Description	Original SubContract Order	Prelims and Method Related Charges		Construction Works				Risk / Opportunity		
			to 30 June 2010	to Completion	Original Subcontract Work	Change		Construction Works Total			
						Already Submitted* and Agreed	Already Submitted* and Not Agreed				New Submission
(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(6 to 9)+(10)	(11)	(4)+(5)+(10)+(11)
1	Preliminaries & General Items	6,169,841	1,678,000	7,393,252						225,000	9,296,252
2	Method Related Charges										
3	Section 1D - All Works	64,076			64,076			(64,076)			
4	Haymarket Viaduct	1,139,067			1,139,067	41,790	76,455	146,276	1,403,587		1,403,587
5	2A Trackwork inc Drainage etc	1,083,745			1,083,745	44,000	556,477	446,022	2,130,243		2,130,243
6	Haymarket Terrace Sub-Station	116,191			116,191			21,964	138,156		138,156
7	Haymarket Station Tramstop	174,548			174,548			27,900	202,448		202,448
8	S20 - Russell Road Underbridge	828,643			828,643		333,542	196,931	1,359,116		1,359,116
9	Russell Road Sub-Station	73,459			73,459			(73,459)	(0)		(0)
10	W18 Retaining Wall	790,597			790,597			317,985	1,108,582		1,108,582
11	5A Trackwork inc Drainage etc	623,008			623,008			805,493	1,428,501		1,428,501
12	Murrayfield Tramstop	284,529			284,529		130,000	75,900	490,429		490,429
13	Roseburn Viaduct	2,853,229			2,853,229			84,447	2,937,676		2,937,676
14	S21B - Murrayfield Ret Walls	1,462,165			1,462,165			615,128	2,077,293		2,077,293
15	S21C - Murrayfield Underpass	190,409			190,409		270,357	12,014	472,780		472,780
16	Schedule 4 Notified Departures							480,182	480,182		480,182
	Total	15,853,507	1,678,000	7,393,252	9,683,666	85,790	1,366,830	3,092,707	14,228,992	225,000	23,525,244

* At the 30th June 2010

Edinburgh Tram Network
 Project Carlisle : "Guaranteed Maximum Price"
 Subcontractor GMP Summary
 Subcontractor : Expanded Ltd

Section 5 : Structures

			GRAND TOTAL							Grand Total	
Item	Description	Original SubContract Order	Prelims and Method Related Charges		Construction Works				Risk / Opportunity		
			to 30 June 2010	to Completion	Original Subcontract Work	Change		Construction Works Total			
(1)	(2)	(3)	(4)	(5)	(6)	Already Submitted* and Agreed	Already Submitted* and Not Agreed	New Submission	(6 to 9)=(10)	(11)	(4)+(5)+(10)+(11)
1	Prelims Section 5	1,521,273	376,281	2,753,544							3,129,825
2	Prelims Edinburgh Pk Bridge	891,811	1,618,258								1,618,258
3	Prelims Carrick Knowe	427,285	316,754	84,724							401,478
4	Prelims W4	168,539	151,686	19,986							171,672
5	Risk									1,466,383	1,466,383
6	S27 Edinburgh Park Bridge	3,388,189			3,388,189		460,456	60,214	3,908,859		3,908,859
7	S23 Carrick Know Bridge	789,453			789,453		160,567	87,978	1,037,998		1,037,998
8	W3 Russell Rd RW						1,736,747	(120,863)	1,615,884		1,615,884
9	W4 Russell Rd Phase 1						430,590	123,675	554,265		554,265
10	W4 Russell Rd Phase 2						3,453,820	10,830	3,464,650		3,464,650
11	S22a&b Balgreen Rd Bridges						1,228,944	(68,734)	1,160,210		1,160,210
12	S32 Depot Access Bridge	1,554,606			1,554,606		1,412,146	350,628	3,317,380		3,317,380
13	A8 Underpass	1,034,444			1,034,444		788,674	29,259	1,852,377		1,852,377
14	S26 South Gyle Access Bridge	632,202			632,202		803,516	77,927	1,513,645		1,513,645
15	S21E Water of Leith Bridge	717,787			717,787		41,767	62,046	821,600		821,600
16	S21D Murrayfield Tp RW	415,965			415,965		462,244	(19,979)	858,230		858,230
17	W8 Baird Drive RW	479,832			479,832		586,866	174,217	1,240,915		1,240,915
18	W9 Balgreen Rd	32,722			32,722		220,117	30,225	283,064		283,064
19	W11 Bankhead Dr RW	81,334			81,334		423,711	(20,038)	485,007		485,007
20	W19 Tramstop RW	209,110			209,110		192,663	13,526	415,299		415,299
21	Service Diversion/support						73,794	229,665	303,459		303,459
22	Discount Exc S27&S23	(136,956)			(136,956)		(118,703)	(17,448)	(273,107)		(273,107)
Section A - Total		12,207,596	2,462,979	2,858,254	9,198,688	73,794	12,284,125	1,003,128	22,559,735	1,466,383	29,347,351

* At the 30th June 2010

Edinburgh Tram Network
 Project Carlisle : "Guaranteed Maximum Price"
 Subcontractor GMP Summary
 Subcontractor : Barr Construction

Section 6 : Depot

			GRAND TOTAL								
Item	Description	Original SubContract Order	Prelims and Method Related Charges		Construction Works				Construction Works Total	Risk / Opportunity	Grand Total
			to 30 June 2010	to Completion	Original Subcontract Work	Change		New Submission			
(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(6 to 9)=(10)	(11)	(4)+(5)+(10)+(11)
1	Preliminaries & General Items	2,137,178	1,643,983	830,413					-		2,474,396
2	Method Related Charges								-		-
3	Earthworks	92,000			92,000	43,843	20,120		155,963		155,963
4	Sub-structure - Depot Building	1,116,433			1,116,433	90,385	86,150		1,292,968		1,292,968
5	Sub-structure - Sub Station Building	57,983			57,983				57,983		57,983
6	Sub-structure - Power Building	55,155			55,155				55,155		55,155
7	Frame	1,160,334			1,160,334	376,398	131,484		1,668,216		1,668,216
8	Upper Floors	102,651			102,651				102,651		102,651
9	Cladding	1,206,649			1,206,649	-	42,620		1,249,269		1,249,269
10	Stairs	8,800			8,800		22,403		31,203		31,203
11	External Blockwork	108,406			108,406	16,613	75,000		200,019		200,019
12	Internal wall and Partitions	241,098			241,098		17,500		258,598		258,598
13	Internal Doors	146,900			146,900		28,508		175,408		175,408
14	Wall Finishes	117,785			117,785				117,785		117,785
15	Floor Finishes	202,247			202,247				202,247		202,247
16	Ceiling Finishes	93,242			93,242		3,000		96,242		96,242
17	Decoration	35,926			35,926				35,926		35,926
18	Fittings and Furnishings	62,182			62,182				62,182		62,182
19	M&E	2,302,000			2,302,000	72,560	158,412	134,805	2,398,167		2,398,167
20	BWIC Services	25,000			25,000				25,000		25,000
21	Site Work	2,880,802			2,880,802	483,198	345,198		3,709,198		3,709,198
22	Sub Station Building	123,035			123,035				123,035		123,035
23	Power Building	109,846			109,846		650		110,496		110,496
24	Sundries	202,565			202,565	149,036	75,565	55,338	371,828		371,828
25	Risk Register	-							-	1,216,063	1,216,063
26	Risk Review Transfer / Deletion							910,363	910,363	- 910,363	-
	Total	12,588,217	1,643,983	830,413	10,451,039	1,232,033	1,006,610	720,220	13,409,902	305,700	16,189,998

* At the 30th June 2010

Edinburgh Tram Network
 Project Carlisle : "Guaranteed Maximum Price"
 Subcontractor GMP Summary
 Subcontractor : Farrans Ltd

Section 5C(part) and 7A

			GRAND TOTAL									
Item	Description	Original SubContract Order	Prelims and Method Related Charges		Construction Works				Risk / Opportunity	Grand Total		
			to 30 June 2010	to Completion	Original Subcontract Work	Change					Construction Works Total	
(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(6 to 9)=(10)	(11)	(4)+(5)+(10)+(11)	
1	Preliminaries & General Items	524,460	477,638	683,880							1,161,518	
2	Method Related Charges	768,308	721,015	1,213,761							1,934,776	
3	Enabling Works	112,091			112,091	593,681	1,339,427		2,045,199		2,045,199	
4	Track and Formation	1,661,460			1,661,460	253,766	1,592,870	(50,963)	3,457,133		3,457,133	
5	Tramstops	363,026			363,026				363,026		363,026	
6	Tramstop Equipment	30,209			30,209				30,209		30,209	
7	Drainage					1,011,137	127,226	18,868	1,157,231		1,157,231	
8	Ducting	379,295			379,295		69,391	(38,554)	410,132		410,132	
9	Highways	101,853			101,853		233,054		334,907		334,907	
10	7A Ingliston P&R Substation	50,586			50,586	1,430			52,016		52,016	
11	7A Gogarburn Bridge S29	885,140			885,140	123,010	48,772	36,670	1,093,592		1,093,592	
12	7A Gogarburn Culvert 1 S30	54,653			54,653	21,145	15,931		91,728		91,728	
13	7A Retaining Walls	337,396			337,396		4,750	3,940,366	4,282,512		4,282,512	
14	7A Culvert 2 S31	113,053			113,053	(22,294)			90,759		90,759	
15	7A Culvert 3 S32	73,408			73,408	6,345			79,753		79,753	
16	OHLE	128,878			128,878	129,131			258,009		258,009	
17	Risk Review Transfer / Deletion							285,000	285,000	120,000	405,000	
	Total	5,583,816	1,198,653	1,897,641	4,291,048	2,117,351	3,431,421	4,191,386	14,031,206	120,000	17,247,500	

* At the 30th June 2010

Edinburgh Tram Network
 Project Carlisle : "Guaranteed Maximum Price"
 Subcontractor GMP Summary
 Subcontractor : Various

Subcontractor : Other Off Street Works

			GRAND TOTAL								
Item	Description	Original SubContract Order	Prelims and Method Related Charges		Construction Works				Construction Works Total	Risk / Opportunity	Grand Total
			to 30 June 2010	to Completion	Original Subcontract Work	Change					
(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(6 to 9)+(10)	(11)	(4)+(5)+(6)-(11)
1	Preliminaries & General Items			1,212,264							1,212,264
2	Method Related Charges			50,000							50,000
3	Track and Formation	2,908,517			2,908,517			3,816,945	6,725,462		6,725,462
4	Tramstops	1,414,483			1,414,483			197,573	1,216,910		1,216,910
5	Utilities works							279,660	279,660		279,660
6	Highways	2,837,172			2,837,172			2,731,149	5,568,321		5,568,321
7	Buildings	291,299			291,299			27,570	318,869		318,869
8	Accommodation Works							73,480	73,480		73,480
9	OHLE	382,187			382,187			75,662	457,849		457,849
10	S27 Edinburgh Park							1,164,295	1,164,295		1,164,295
11	A8 Underpass							1,753,699	1,753,699		1,753,699
12	S26 5th Gyle Access Bridge							125,456	125,456		125,456
13	S21E Water of Leith							430,000	430,000		430,000
14	W4 Russell Rd RTW							1,061,011	1,061,011		1,061,011
15	Other Structures							391,844	391,844		391,844
16	Miscellaneous							2,814,775	2,814,775		2,814,775
17	Off Street TM	1,010,745	756,184	2,065,216				-	-		2,821,400
18	Depot							1,350,497	1,350,497		1,350,497
19	Depot to Airport							223,298	223,298		223,298
	Total	8,844,403	756,184	3,327,480	7,833,658	-	-	16,121,767	23,955,425	-	28,039,089

* At the 30th June 2010

Edinburgh Tram Network
 Project Carlisle : "Guaranteed Maximum Price"
 Subcontractor GMP Summary
 Subcontractor : Parsons Brinkerhoff

Design

Item	Description	Original SubContract Order
1	PH III Detailed Design Deliverables	2,204,792
2	PH IV Construction Phase	1,104,023
3	Extended Construction Support	1,000,000
4	Design Support Team	675,000
5	Changes	-
6	Additional Technical Resources (ATRs)	-
7	Incentivisation	
8	Prolongation	
9	Design Coordination and Support	
10	Disruption Claim	
	Total	4,983,815

Original Subcontract Work	Scope Works			Scope Works Total	Risk / Opportunity	Grand Total
	Change					
	Already Submitted* and Agreed	Already Submitted* and Not Agreed	New Submission			
2,204,792				2,204,792		2,204,792
1,104,023			466,926	1,570,949		1,570,949
1,000,000	672,897		2,180,425	3,853,322		3,853,322
675,000	41,423		141,912	858,335		858,335
-	3,189,303	271,977	1,082,493	4,543,772		4,543,772
-	158,301		150,000	308,301		308,301
				-		-
	250,000	541,890	176,302	968,192		968,192
	495,000			495,000		495,000
			1,045,052	1,045,052		1,045,052
4,983,815	4,806,924	813,867	5,243,111	15,847,716	-	15,847,716

* At the 30th June 2010

APPENDIX 1.2

SIEMENS - Guaranteed Maximum Price Breakdown

Appendix 1.2 - Siemens GMP Breakdown

		Carlisle
Siemens UK	UK Project Management	13,324,667
Siemens UK	UK System Engineering	3,001,626
Siemens UK and BAM	Trackwork	43,471,285
Siemens UK	UK Depot Workshop	2,028,342
Siemens UK	UK Electrification	6,003,202
Siemens UK	Infrastructure	3,185,035
Siemens UK	Insurance, Bonds and Financial Guarantees	1,712,358
Siemens UK	Control & Information	5,111,939
Siemens UK	Communications	5,009,483
Siemens AG	Electrification, Automation and Depot Equipment	29,688,648
	Change Orders	5,123,140
	Additional Carlisle Components	941,496
Overall Project	Total	118,601,221

APPENDIX 1.3

CAF - Guaranteed Maximum Price Breakdown

Appendix 1.3--CAF GMP Breakdown

CAF cost associated with Project Carlisle

Assumptions:

- The Delay Costs below are generated by delays incurred by the project and formalized with Project Carlisle. They include the claims presently held by CAF for other delay claims (MUDFA, ETN, etc.). There would be no double recovery, so if Project Carlisle would be retained by Tie, the other claims for delay generated costs would be settled with these amounts.
- To obtain the total amount that would be due to CAF, we add two components: Delay Costs, as explained above, and normal Contractual Payments pending as of 23 July 2010 (certified milestones or not, all payments until completion of TSA).
- CAF has assumed Tie will continue to pay Tram Delivery Milestones as it is doing today. CAF has assumed the Commissioning Milestones will not be paid until the infrastructure allows Commissioning tests to occur, and therefore they will be delayed, incurring Delay Costs.
- CAF has analyzed the Infrastructure availability during the Commissioning, and presently understands that the rate in the Contractual Programme (three trams per month commissioned) cannot be sustained under the precarious conditions associated with Infrastructure availability at the first stages of Project Carlisle. We understand that the rate must be a maximum of 2 tramways per month, and the Programme would need to be updated to reflect that rate. Nevertheless we estimate that enough trams can be commissioned so no delay is introduced in the start of service for that reason. As required by the Consortium in P. Carlisle, these delays in tram commissioning should not generate LDs to CAF (as the tramways are being delivered to date and are available for commissioning as contractually required).
- The cost of the maintenance of tramways will decrease due to less kilometres of service per year, while the line is not completed. We estimate the number of kilometres will amount to a quantity smaller than what is foreseen in the TMA, and therefore needs to be agreed, logically the cost per kilometre will be higher than the costs agreed, although the total Maintenance Cost according to the TMA will be smaller. CAF has not the information to estimate that TMA price reduction (year – kms targeted by Tie under P. Carlisle), and it is not included in the amounts below.
- The Delay costs have been estimated according to the Programme Project Carlisle Revision 00, (100729).

Delay costs:

OVERALL COST	COSTS	
Warranties	698.317,75	euros
Technical office	104.000,00	euros
Consultancy firms	77.328,00	euros
Project Management	254.338,28	euros
Maintenance	448.283,33	euros
Financial impact	600.991,79 €	euros
Foreign Exchange	67.199,51	euros
Legal Department	46.800,00	euros
Comercial Department	46.800,00	euros
TOTAL 1	2.344.058,67	euros
Cost at CAF IRUN for UT3-UT14	COSTS	
Parking at CAF IRUN	813.713,27	euros
Security for the trams	21.276,00	euros

Insurances	45.742,40	euros
Additional Couplings & Uncoupling	0,00	euros
Normal Maintenance	4.520,00	euros
Long Term Maintenance	292.428,00	euros
Total 2	1.177.679,67	euros
Costs at RENFE IRUN for UT3-UT14	COSTS	
Parking at RENFE IRUN	622.079,58	euros
Security for the trams	217.618,46	euros
Insurances	258.956,34	euros
Additional Couplings & Uncoupling	48.738,00	euros
Normal Maintenance	25.588,57	euros
Long Term Maintenance	0,00	euros
Total 3	1.172.980,95	euros
COST at CAF IRUN for UT15-UT27	COSTS	
Parking at CAF IRUN	0,00	euros
Security for the trams	69.738,00	euros
Insurances	324.476,11	euros
Additional Couplings & Uncoupling	0,00	euros
Normal Maintenance	32.062,86	euros
Long Term Maintenance	316.797,00	euros
Total 4	743.073,97	euros
Grand Total (1+2+3+4) Delay Costs	5.437.793,27	Euros

TSA Pending payments:

TSA pending payments as of 27th July 2010: £13.074.780,34

APPENDIX 1.4

SDS - Guaranteed Maximum Price Breakdown

Appendix 1.4 - SDS GMP Breakdown

SDS ref: ULE90390A Core Scope

Date from	Date to	Description	Comments	Value
15 May 2008	01 November 2010	Phase III works outstanding	10% of late deliverables - secured when DAS issued	£ 36,000.00
15 May 2008	01 November 2010	Phase III works outstanding	Systems Assurance deliverables	£ 193,011.00
15 May 2010			Adjustment for MUDFA contract	-£ 14,603.00
15 May 2010			Adjustment for DCR0114	£ 5,605.00

Total £ 220,013.00

Dates above relate to V 60 of the SDS Design Delivery Programme

CEC00218042_0035

SDS ref: ULE90390A Changes

Date from	Date to	Description	Comments	Value
15 May 2008	01 June 2010	BSC Change Instructed (DCO)	Remainder of fee for instructed changes	£ 866,246.09
15 May 2008	01 June 2010	BSC Change Estimated	Changes not yet instructed	£ 1,032,127.25
15 May 2010			Adjustment for DCR0114	-£ 5,605.00

Total £ 1,892,768.34

SDS ref: ULE90485A DC - Design Coordination

Date from	Date to	Description	Comments	No. of Weeks	Rate	Value
17 January 2010	31 January 2010	Remainder of DC monies	Period 4 & Period 5			£ 141,428.57

Total £ 141,428.57

CEC00218042_0037

SDS ref: ULE90391A Phase IV

Date from	Date to	Description	Comments	No. of Weeks	Rate	Value
18 June 2010	15 July 2011	Fee for Phase IV - As Builts	Pro Rata 25.6k per period Plus £50k Post Construction Surveys			£ 409,035.00
19 July 2010	01 November 2010	Jason Chandler		37.5	15 £ 95.00	£ 53,437.50
02 November 2010	31 May 2012	Jason Chandler		7.5	82 £ 95.00	£ 58,628.57
19 July 2010	01 November 2010	Kate Shudall		37.5	15 £ 95.00	£ 53,437.50
02 November 2010	21 December 2011	Kate Shudall		15	59 £ 95.00	£ 84,278.57
19 July 2010	01 November 2010	Claire Paterson		37.5	15 £ 38.00	£ 21,375.00
19 July 2010	30 May 2012	Pauline Benn		37.5	97 £ 38.00	£ 138,632.14
15 July 2011	30 May 2012	Colin Cromar		37.5	46 £ 49.00	£ 84,000.00
19 July 2010	26 November 2010	Martin Alexander		4	19 £ 78.00	£ 5,794.29
19 July 2010	26 November 2010	Brian McCrear		24	19 £ 95.00	£ 42,342.86
Credit for As Built Drawings Yet to Be Completed						-£ 75,000.00

Total £ 875,961.43

- Jason Chandler Full time Project Management during completion of the detailed design and then 1 day per week until the end of the commission
- Kate Shudall Full time commercial support during completion of the detailed design and then 2 day per week until the end of the commission preparing invoices, ATRs, change estimates.
- Claire Paterson Secretarial and office management support to the completion of the SDS detailed design
- Pauline Benn Document control support until the end of the SDS commission
- Colin Cromar As built drawing preparation and CAD support
- Martin Alexander Updating of Design Programme until end of detailed design completion
- Brian McCrear Halcrow PM cover until end of detailed design.

CEC00218042_0038

SDS ref: ULE90392A ECS - Extended Construction Support

Date from	Date to	Description	Comments	No. of Weeks	Rate	Total
01 July 2010	01 November 2010	Alan Dolan - Time split with Design Support until 1/11/10	28	18	£ 95.00	£ 46,740.00
01 July 2010	01 November 2010	Claire Ferguson - Time split with Design Support until 1/11/10	28	18	£ 38.00	£ 18,696.00
01 November 2010	25 May 2012	Alan Dolan	37.5	82	£ 95.00	£ 290,598.21
01 November 2010	25 May 2012	Claire Ferguson	37.5	82	£ 38.00	£ 116,239.29
01 July 2010	25 May 2012	Chris Reid	37.5	99	£ 95.00	£ 353,196.43
01 July 2010	27 May 2011	Tom Kelly	37.5	47	£ 95.00	£ 167,946.43
01 July 2010	29 July 2011	Scott Kelland	37.5	56	£ 78.00	£ 164,217.86
01 July 2010	31 August 2012	Jacek Stachowiak	37.5	113	£ 49.00	£ 207,900.00
01 July 2010	01 January 2011	Steve Anderson	37.5	26	£ 78.00	£ 76,885.71
01 July 2010	01 May 2012	Stephen Surtees	37.5	96	£ 95.00	£ 340,982.14
01 July 2010	28 October 2010	Tom Culligan	37.5	17	£ 78.00	£ 49,725.00
01 July 2010	01 May 2011	Kevin McGinty	37.5	43	£ 78.00	£ 127,028.57
As required	2 months	Roads Engineer - Lindsey Road and Depot access road	37.5	8	£ 95.00	£ 28,500.00
01 July 2010	29 July 2011	Structures Engineer	37.5	56	£ 78.00	£ 164,217.86
21 June 2010	30 June 2010	See Latest Application				£ 27,551.50

Total **£ 2,180,425.00**

SDS ref: ULE90390A Disruption

Date from	Date to	Description	Comments	Value
01 January 2008	30 December 2008	Net of EOT		£ 316,902
01 January 2009	30 December 2009	Net of EOT		£ 503,149
01 January 2010	30 June 2010	Net of EOT		£ 125,000
				£ 100,000

Total £ 1,045,051.00

SDS ref: ULE90474A Prolongation

Date from	Date to	Description	Comments	Value
15 May 2008	26 September 2009	PB and Halcrow Prolongation Claim Up to 26th Sept '09 only	£50k of £250k outstanding as per MoU	£ 50,000.00
15 May 2008	26 September 2009	PB and Halcrow Prolongation Claim Up to 26th Sept '09 only	£792k of total Claim minus £250k accepted under MoU	£ 541,890.00
26 September 2009	02 November 2009	PB and Halcrow Prolongation Claim Up to 02nd Nov '09 only	EOT bewteen 26th Sept '09 and 02 November '09	£ 60,252.00
28 February 2010	16 July 2010	PB and Halcrow Prolongation Claim Up to 16th July '10 only	EOT bewteen 28th Feb '10 and 16th July '10	£ 116,050.46

Total £ 768,192.46

Summary of supporting information
 15 May 2008 to 28 February 2009 Detailed in SDS Contract Additional Managment and Supervision Document of 31 May 2009
 02 March 2009 to 25 September 2009 Detailed in e mail from Steve Reynolds to martin Foerder on 29/09/2009 13:25
 Total of above £791,890 for the period May 2008 to 25 September 2009
 26 September 2009 to 2 November 2009 Detailed on Tab labelled Prolongation 26-9-09 to 2-11-09
 28 February 2010 to 16 July 2010 Detailed on Tab labelled Prolongation 28-2-10 to 16-7-10

CEC00218042_0042

SDS ref: ULE90390A Incentivisation

Date from	Date to	Description	Comments	Value
15 May 2008	Present	Incentivisation	Issued on time	£ -
15 May 2008	Present	Incentivisation	Delayed due to tie/CEC Delay	£ -
This issue has been removed from the Revised Project Carlisle Proposal, as that it is contracted directly between tie and SDS.				

Total £ -

CEC00218042_0043

EDINBURGH TRAM NETWORK - BSC CHANGE INSTRUCTED

Index	NTC (BSC) Ref to file	Short Title	By BOS			From BOS			Adjustment due to DC	Certified	Fee Remaining	Cost for Drawings		
			Change Estimate			Change Order								
Index	NTC (BSC) Ref to file	Short Title	Number	Date	Value	Number	Date	Value	DC Change?	Certified	Fee Remaining	Challenged by file		
DCR0024	VE	Roeburn Street Vackel VE	DCR0024	03/06/2009	£ 316,619	DCO011 (rev DCO09)	06/02/2009	£ 316,619		£ -	£ 284,637.10	£ 31,661.90		
DCR0082	76	Signal Additional Transition	DCR0082	11/05/09	£ 346,038	DCO053	16/06/2009	£ 346,039		£ -	£ 282,522.26	£ 78,415.74		
DCR0082A		Signal Additional Transition - Urban requirements	DCR0082A	20/03/10	£191,514.31	DCO121	24/06/2010	£ 169,877		£ -	£ 62,609.90	£ 27,937.95	£ 628.00	
DCR0191	278	Wong Awa Motorway	DCR0191	23/02/2009	£ 8,200	DCO060	26/07/2009	£ 4,875	✓	£ -	£ 8,873.00	£ 1,623.00		
DCR0192	148	NO CEC comment to make	DCR0192	27/02/2009	£ 19,393	DCO067	09/01/2010	£ -	✓	£ -	£ -	£ -		
DCR0104	143	Loc made up to adaptable standards	DCR0104	16/07/2008	£ 34,132	DCO071	23/09/2008	£ 34,132		£ -	£ 30,718.80	£ 3,413.20		
DCR0125	269	Tracklens - ground improvement	DCR0125	29/01/2009	£ 274,666	DCO018	23/02/2009	£ 274,666		£ -	£ 239,995.10	£ 24,720.45		
DCR0127	244	Building Fixing Overhead Area masts	DCR0127	29/01/2009	£ 14,285	DCO090	09/12/2009	£ 14,285	✓	£ -	£ -	£ 14,265.00		
DCR0134	268	NSL Timber & steel poles	DCR0134	24/02/2009	£ 40,342	DCO099	20/07/2009	£ 40,342		£ -	£ 34,490.28	£ 6,091.22		
DCR0140	271	Roads construction analysis of 2009/10	DCR0140	29/01/2009	£ 289,440	DCO220	23/02/2009	£ 289,440		£ -	£ 89,615.20	£ 200,624.80		
DCR0141	283	Thameside Vale gateway	DCR0141	11/09/2009	£ 4,894	DCO381	10/12/2009	£ 4,894	✓	£ -	£ 1,769.50	£ 3,124.10		
DCR0146	277	Design of canopy and boundary treatment at Airport Road	DCR0146	28/02/2009	£ 42,574	DCO058	15/05/2009	£ 42,574		£ -	£ 38,099.20	£ 6,514.80		
DCR0147	287	Forth Park Access rd traffic sign	DCR0147	26/02/2009	£ 7,778	DCO062	28/07/2009	£ 7,778		£ -	£ 7,600.20	£ 777.80		
DCR0160		Design of canopy and boundary treatment at Airport Road	DCR0160	17/04/2009	£ 14,632	DCO058	17/06/2009	£ 14,632		£ -	£ 14,632.00	£ -		
DCR0167	281	Alternative design - Henderson Park	DCR0167	30/10/2009	£ 119,171	DCO084	10/12/2009	£ 119,171	✓	£ -	£ 20,000.00	£ 99,171.00	£ 5,625.00	
DCR0168		Design of canopy and boundary treatment at Airport Road	DCR0168	23/04/2009	£ 41,304	DCO044	09/06/2009	£ 41,304		£ -	£ 33,043.80	£ 8,260.40		
DCR0170	270	RMS Transition Existing Utility	DCR0170	02/09/2009	£ 9,775	DCO378	03/12/2009	£ 1,665	✓	£ -	£ 339.00	£ 1,332.00		
DCR0172	272	RAA New Dundee Road	DCR0172	21/02/2009	£ 9,311	DCO029	20/02/2009	£ 9,311		£ -	£ 1,655.90	£ 7,655.10		
DCR0178	279	RAA New Dundee Road	DCR0178	01/05/2009	£ 6,266	DCO091	04/05/2009	£ 6,266		£ -	£ 1,545.00	£ 4,895.00		
DCR0179	279	RAA New Dundee Road	DCR0179	21/02/2009	£ 47,262	DCO074	24/02/2009	£ 47,262		£ -	£ 12,533.80	£ 4,726.20		
DCR0181	302	Signal Additional Transition	DCR0181	23/02/2009	£ 46,851	DCO086	06/01/2010	£ 25,139	✓	£ 11,113	£ 35,198.00	£ 11,713.00		
DCR0191	310	Signal Additional Transition	DCR0191	05/06/2009	£ 16,284	DCO083	16/12/2009	£ 16,284	✓	£ -	£ 14,655.60	£ 1,828.40		
DCR0193		Signal Additional Transition	DCR0193	08/12/2009	£ 2,128	DCO105	06/01/2010	£ 380	✓	£ -	£ -	£ 380.00		
DCR0198	492	Signal Additional Transition	DCR0198	20/02/2009	£ 12,113	DCO084	30/07/2009	£ 20,000	✓	£ 10,887	£ 12,113.00	£ -		
DCR0198	417	RAA Dunning Future parking	DCR0198	27/06/2009	£ 1,112	DCO098	10/12/2009	£ 1,112	✓	£ -	£ 789.52	£ 353.48		
DCR0200	430	RAA New Dundee Road	DCR0200	01/10/2009	£ 5,818	DCO089	10/12/2009	£ 5,818	✓	£ -	£ 3,409.25	£ 2,334.25	£ 879.00	
DCR0202	463	RAA New Dundee Road	DCR0202	10/12/2009	£ 1,271	DCO089	10/12/2009	£ 4,500	✓	£ -	£ 4,500.00	£ -		
DCR0203	461	RAA New Dundee Road	DCR0203	10/12/2009	£ 1,250	DCO086	10/12/2009	£ 10,000	✓	£ -	£ 1,250.00	£ 2,782.50	£ 362.00	
DCR0205	465	RAA New Dundee Road	DCR0205	10/12/2009	£ 3,262	DCO087	10/12/2009	£ 2,000	✓	£ -	£ 1,262	£ 2,000.00	£ 1,261.50	£ 42.50
DCR0209	305	RAA New Dundee Road	DCR0209	05/03/2010	£ 20,164	DCO101	06/01/2010	£ 18,750	✓	£ 1,414	£ 18,750.00	£ 1,414.00		
DCR0212A	476	RAA New Dundee Road	DCR0212A	28/08/2010	£ 116,773	DCO106	06/01/2010	£ 26,250	✓	£ 89,523	£ 23,625.00	£ 93,148.00	£ 3,570.00	
DCR0213A	477	RAA New Dundee Road	DCR0213A	13/11/2009	£ 139,850	DCO111	15/01/2010	£ 67,500	✓	£ 72,450	£ 57,735.00	£ 82,215.00	£ 892.50	
DCR0214		RAA New Dundee Road	DCR0214	11/09/2009	£ 11,397	DCO103	06/01/2010	£ 6,518	✓	£ 2,839	£ 7,665.20	£ 3,890.00		
DCR0218	488	RAA New Dundee Road	DCR0218	21/12/2009	£ 28,235	DCO109	06/01/2010	£ 18,250	✓	£ 20,585	£ 15,937.50	£ 22,297.50	£ 2,635.00	
DCR0219	498	RAA New Dundee Road	DCR0219	25/02/2010	£ 11,293	DCO110	06/01/2010	£ 11,290	✓	£ 163	£ 1,590.00	£ 3,894.50		
DCR0220	512	RAA New Dundee Road	DCR0220	04/11/2009	£ 10,007	DCO104	06/01/2010	£ 7,250	✓	£ 2,757	£ 3,750.00	£ 6,254.50	£ 352.00	
DCR0227	376	RAA New Dundee Road	DCR0227	06/01/2010	£ 137,948	DCO114	16/03/2010	£ 60,000	✓	£ 77,848	£ 54,000.00	£ 83,947.50	£ 4,250.00	
					£ 2,350,284					£ 2,048,628.90	£ 277,591.60	£ 1,451,689.31	£ 668,248.09	£ 19,510.50

EDINBURGH TRAM NETWORK - BSC CHANGE ESTIMATED

Index	NTC (BSC) Ref to file	Short Title	Section	By SDS			From BBS			Certified	Fee Remaining	Cost for Drawings
				Change Estimate Number	Date	Value	Change Order Number	Date	Value			
DCR0062	61	Goggs Surchage Utiliation	7A	DCR0062	01/06/2008	£ 1,994				£ 1,994.00		
DCR0196		Internal Consultation THO Changes	SW	DCR0196	31/07/2009	£ 77,940				£ 77,940.00		
DCR0226		OLE Base Special - Airport	7A	DCR0226	31/03/2010	£ 27,890			£ 5,578.00	£ 22,311.50	£ 297.50	
DCR0229		OLE Pinhs on Structures	SW	DCR0229	08/04/2010	£ 115,526				£ 115,525.75		
DCR0233	529	Gogar Landfill profile re design	7A	DCR0233	10/03/2010	£ 19,875			£ 19,874.50	£ -	£ 263.50	
DCR0235		Special Trackform on Edinburgh Railway Bridge	1B	DCR0235	08/04/2010	£ 8,220				£ 8,228.75	£ 42.50	
DCR0237		Retaining Track Design	SW	DCR0237	08/04/2010	£ 68,510				£ 68,510.00		
DCR0238		Hydraulic Model of Gogarburn Retaining Walls	7A	DCR0238	10/03/2010	£ 57,980				£ 57,980.00		
DCR0241		Topo Survey ScotRail Depot	5A	DCR0241	02/03/2010	£ 1,843			£ 1,842.50	£ -		
DCR0242	167	Changes to Murrayfield Accommodation Works - Wanderers Club House	5A	DCR0242	15/03/2010	£ 992				£ 992.00		
DCR0248		Special Trackform Construction at Shallow Depth obstructions	1	DCR0248	14/05/2010	£ 61,695				£ 61,694.50		
DCR0250		Further to DCR0113 amendments are required to Internal Retaining Wall for Haymarket Viaduct	2A	DCR0250	13/05/2010	£ 3,806			£ 3,806.00	£ -		
DCR0251		Reinstatate traffic island on Princes St	1C	DCR0251	04/05/2010	£ 1,140				£ 1,140.00		
DCR0253		Husker Road Retaining Wall Installation Error	5A	DCR0253	24/05/2010	£ 23,757				£ 23,757.00		
DCR0255		Groveport Street 2 Way Change	1D	DCR0255	04/05/2010	£ 49,980				£ 49,980.00	£ 850.00	
DCR0256		Canalbank Lane Substation Measurement	1C	DCR0256	25/05/2010	£ 23,114			£ 20,000.00	£ 3,113.50		
DCR0257		Husker Road Retaining Wall - Review ground conditions W3	5A	DCR0257	04/05/2010	£ 7,411				£ 7,411.00		
DCR0259		Special Trackform Construction at Shallow Depth obstructions	1	DCR0259	20/05/2010	£ 18,720				£ 18,720.25		
Estimated but not yet formally submitted/reviewed by BSC												
DCR0124A	358	NL further future proofing - OLE changes	7A			£ 4,378				£ 4,378.00		
DCR0245		OLE Special Foundations - Several	SW			£ 105,438				£ 105,437.50		
DCR0283		IQ1853 Update SB MX file	5B			£ 3,833				£ 3,832.50		
DCR0265	485	Tram Turnback Strategy	SW			£ 80,000				£ 80,000.00		
DCR0271		OLE review of LAF from Close out of OLE analysis and issue of	SW			£ 7,733				£ 7,732.50		
DCR0272		Haymarket OLE Pinhs	2A			£ 1,883				£ 1,882.50		
DCR0274		Murrayfield Stadium Retaining Walls Ground improvement	5A			£ 52,400				£ 52,400.00		
DCR0222		Change IQ1909 Street Lighting now connect	1D			£ 6,683				£ 6,682.50		
DCR0230		Control aluminium ducting inc on Structures				£ 57,600				£ 57,500.00		
DCR0231		Setters additional requirements Lighting and Bonding Development Workshop	SW			£ 73,157				£ 73,157.00		
DCR0284		Obtained Building Consents for Dublin St	1C			£ 11,250				£ 11,250.00		
DCR0267		Depot Tram Wash Equipment	6			£ 26,343				£ 26,343.00		
DCR0268		Depot Review layout showing service requirements				£ 10,005				£ 10,005.00		
DCR0269		Depot Lighting and Bonding and Special Sub-stations				£ 14,105				£ 14,105.00		
DCR0270		Tramstop Layout Changes	SW			£ 23,887				£ 23,887.50		
DCR0273	586	Junction of the Mound, Princes St and Hanbury St	1C			£ 4,556				£ 4,556.00		
DCR0277		CERC railway (existing and Bonding)	7A			£ 10,000				£ 10,000.00		
DCR0262		Additional Cross Sections for Depot Formation Design	6			£ 8,000				£ 8,000.00		
DCR0283		OLE updates for schedules of Bases - Revision F emailed to SDS	SW			£ 12,000				£ 12,000.00		
						£ 1,087,528				£ 50,401.00	£ 1,032,127.25	£ 1,453.50

* Only Changes listed above (plus previously instructed) are included in the GMP fee
 * All above changes are estimated relevant to the associated Change Notices/Letters/Email correspondence between SDS & BSC

EDINBURGH TRAM NETWORK - CHANGES MAY OR MAY NOT BE REQUIRED BUT SCOPE YET TO BE CONFIRMED FOR THE PURPOSE OF CHANGE ESTIMATION AND INSTRUCTION

Index	NTC (BSC) Ref to the	Short Title	Full Description	From RB3		By SD3			Certified	Fee Remaining	Comments
				Change Notification		Change Estimate					
Index	NTC (BSC) Ref to the	Short Title	Full Description	Reference	Date	Number	Date	Value	Certified	Fee Remaining	Comments
DCR0144		Stage E Landscape Drawings	As per CEC Alternative and existing Stage E						E	-	-
DCR0228		South Gyle Access Bridge Detailed Design	Changes in 320 and W11 detailed design						E	-	-
DCR0232		Forer Place Bridge - Remove Ramp, Remove concrete Sowerstand, Revise OLE Baseplate	See RDC1154	RDC154 RDC154A	21/01/2010 02/07/2010				E	-	-
DCR0240		Forer Place Section 1A - Fence Advance	Additional constraints in Casino area						E	-	-
DCR0243		Advanced Directional Signing for Construction 06	New CEC comment related to the provision of additional signs, not previously discussed						E	-	-
DCR0244		Lambay Rd phone mast	New phone mast construction conflicts with proposed structure design						E	-	-
DCR0252		Mounting of sign plates on pylons	Letter request to review all possibilities						E	-	-
DCR0291		Reservoir St Viaduct curtain walls	Design of curtain walls required at Reservoir St in lieu of current fencing proposals in the vicinity of each abutment	RDC167	20/05/2010				E	-	-
DCR0296		Murrayfield Tramstop layout Change	Boilard, Fence and Turnstile Changes						E	-	-
DCR0276		Eden Street Junction	Redesign Junction as requested by CEC	RDC170	06/07/2010				E	-	-
DCR0278		Forer Place Detailed Design	Further to DCR0173 & DCR0221						E	-	-
DCR0280		Prohibition Quayside Change							E	-	-
DCR0281		Signage interchange - Name Change	Edinburgh Gateway						E	-	-
DCR0282		Signage interchange - All Drainage	Relocate combined sewer						E	-	-
									E	-	-

EOT bewteen 26th September '09 and 2nd Nov '09

20/07/2010

Employee/Supplier	Data	Week					Total
		28/09/2009	05/10/2009	12/10/2009	19/10/2009	26/10/2009	
CHANDLER, JASON ROY	Sum of Quantity						
	Average of Contract Rate						
	Sum of Contract Estimate						
CHANDLER, Mrs. CARLA (CARLA)	Sum of Quantity						
	Average of Contract Rate						
	Sum of Contract Estimate						
SHUDALL, KATE	Sum of Quantity						
	Average of Contract Rate						
	Sum of Contract Estimate						
	Sum of Quantity						
	Average of Contract Rate						
	Sum of Contract Estimate						
	Sum of Quantity						
	Average of Contract Rate						
	Sum of Contract Estimate						
Total Sum of Quantity							
Total Average of Contract Rate							
Total Sum of Contract Estimate							

Halcrow

Brian McCrear			
Laurie Mentiplay			

Total £ 60,252.00

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Employee	Data	Week
CHANDLER, ANSON ROY	Sum of Quantity	
	Average of Contract Rate	
	Sum of Contract Estimate	
SHOULL, KATE	Sum of Quantity	
	Average of Contract Rate	
	Sum of Contract Estimate	
	Sum of Quantity	
	Average of Contract Rate	
	Sum of Contract Estimate	
MARTIN, ALEXANDER	Sum of Quantity	
	Average of Contract Rate	
	Sum of Contract Estimate	
	Planning Support	
	Planning Support	
	Sum of Quantity	
	Average of Contract Rate	
	Sum of Contract Estimate	

Microrow

Sum of Quantity	45710
Sum of Contract Estimate	118051.46

Management of completion on SDS Phase B weeks

APPENDIX 2

VALUATION OF TIE CHANGES AND PROLONGATION

1 TIE CHANGES

- 1.1 We propose that amendments to the existing Schedule Part 4 Appendix G would be appropriate to simplify the rules concerning valuation of tie Changes and to establish rates for valuation of prolongation.
- 1.2 The valuation of any tie Change will continue to be made in accordance with Clause 80.6.
- 1.3 Where Clause 80.6.3 and/or 80.6.4 applies and to the extent that they apply to a tie Change then the valuation shall be on the basis of Actual Cost or Estimated Actual Cost.
- 1.4 In respect of a valuation of any work under 1.3 above **tie's** Representative shall apply head office overheads and profit percentages to the appropriate elements of Actual Cost or estimated Actual Cost as follows:

(a)	Civil Engineering works	10%
(b)	Systems and Track works	17%

- 1.5 In all cases where a **tie** Change is being valued in accordance with Clause 80.6, Site related overhead (Preliminaries) shall be valued and added as follows:

7.4% to be added to the net valuation of the tie Change to cover the Consortium Preliminaries.

17.5% to be added to the net valuation of the tie Change to cover any other Preliminaries with regard to any **tie** Change associated with Civil Engineering Works, provided that this calculation shall in no case apply to Systems and Trackwork or claims for other Preliminaries in relation to prolongation costs arising from extensions of time or delay.

Site related overhead (Preliminaries) in respect of Systems and Trackwork and in respect of Tram Supply Obligations shall be valued in accordance with paragraph 2 (Prolongation) below.

2 PROLONGATION

- 2.1 Infraco acknowledges that the principle of valuation of prolongation costs arising from extensions of time granted has been established through the mediation process leading to agreement of the Estimate for Infraco Notification of tie Change Number 1. The tie Change Order number 116 reflects the basis of this agreement, which shall be formalised and applied to the valuation of prolongation costs arising from extensions of time. Infraco proposes that:

- (a) Prolongation costs including Preliminaries elements relating to Systems and Trackwork shall be based on Actual Cost or Estimated Actual Cost of the resources and/or labour employed by Infraco or its sub-contractors.

- (b) In respect of the valuation of cost elements relating to Systems and Trackworks, 17% is to be added to the Actual Cost or Estimated Actual Costs in respect of related head office overheads and profit percentages.
- (c) Preliminaries elements in relation to prolongation costs which relate to Civil Engineering Works arising from extensions of time or delay, valued in accordance with the rates set out in Appendix 2.1 hereto.
- (d) In all cases where prolongation is being valued in accordance with Clause 80.6 costs which relate to the Tram Supply Obligations shall be valued on the basis of Actual Cost or estimated Actual Cost.
- (e) In relation to Systems and Trackwork Actual Cost or Estimated Actual Cost will be established on the basis of the principles agreed in relation to tie Change number 1.

APPENDIX 2.1

PROLONGATION CALCULATION WORKSHEET

See Supplemental Information CD provided with 29 July 2010 Project Carlisle Proposal;

Appendix 2.1 – Prolongation Calculation Worksheet

APPENDIX 3

GMP DRAWINGS

For the section of tram between Airport and Haymarket, and other agreed accommodation works.

3.1 BSC GMP Drawings

See Project Carlisle BBUK GMP – See CD provided with 29 July 2010 Project Carlisle Proposal:

The following attached drawings supersede those previously provided on the Project Carlisle BBUK GMP CD provided with 29 July 2010 Project Carlisle Proposal:

ULE90130-07-BRG-00083, REV 1
ULE90130-05-PLG-00265, REV2
ULE90130-07-DRG-00136, REV 5
ULE90130-07-DRG-00137, REV5
ULE90130-07-STP-00032, REV 2
ULE90130-07-HRL-00003, REV 10
ULE90130-07-HRL-00903, REV 9
ULE90130-07-HRL-00403, REV 7
ULE90130-07-HRL-00503, REV 8
ULE90130-07-HRL-00201, REV 9

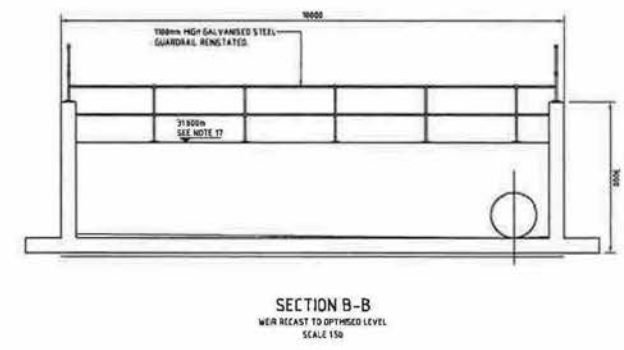
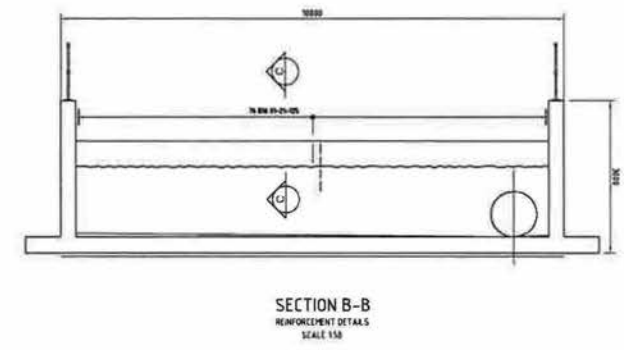
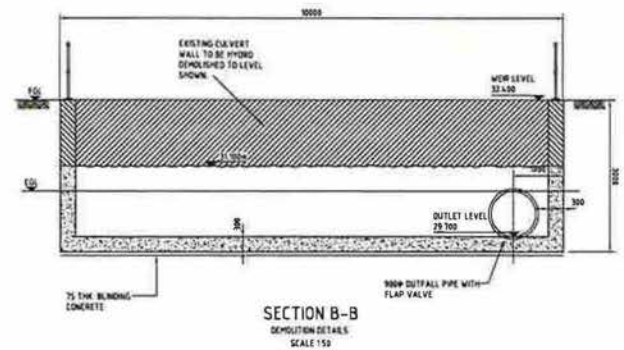
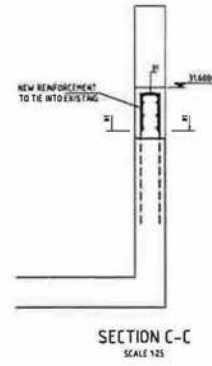
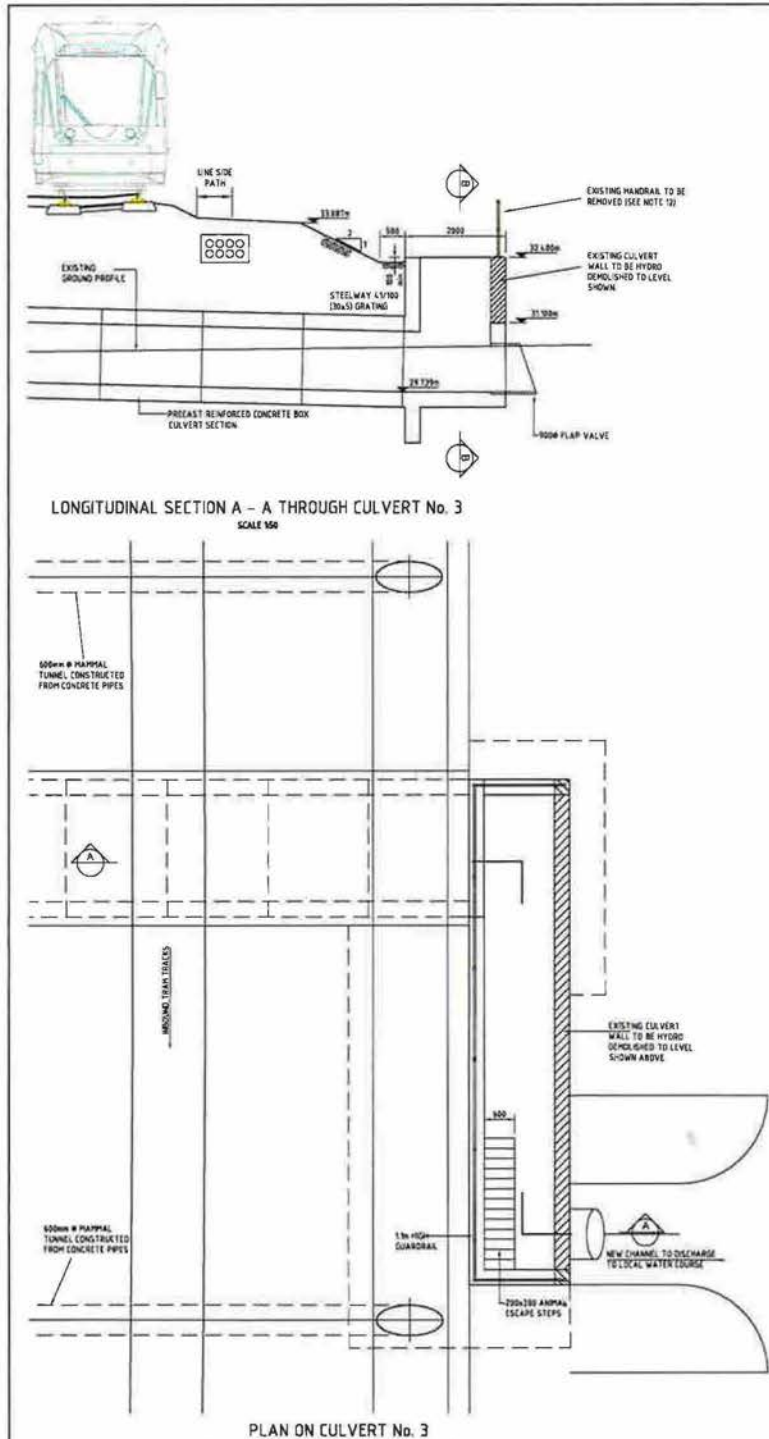
Appendix 3.1a – GMP Drawing Register 1 - See CD provided with 29 July 2010 Project Carlisle Proposal:

Appendix 3.1b – GMP Drawing Register 2 - See CD provided with 29 July 2010 Project Carlisle Proposal:

Appendix 3.1c – GMP Drawing Register 3 - See CD provided with 29 July 2010 Project Carlisle Proposal:

3.2 Siemens Design Submissions

See Carlisle Siemens Appendix 3 – See CD provided with 29 July 2010 Project Carlisle Proposal:



- NOTES**
- This drawing to be read in conjunction with the relevant Specification and all other relevant drawings.
 - All dimensions and levels to be checked on site and any discrepancies identified prior to commencement of work.
 - All dimensions in millimetres unless otherwise noted.
 - All levels in metres unless otherwise noted.
 - All concrete to be Grade C10/150 (Mk 3) Blinding to be grade S17.
 - All reinforcement to be Grade B500 in accordance with BS4449 2005.
 - All cover to reinforcement to be in accordance with BS4500.
 - All exposed concrete surfaces shall be impregnated with paint.
 - Max. water cement ratio shall be 0.45.
 - All steelwork (including girders) should be reviewed for touch potential. Consideration should be given to electrical continuity between parallel units. Reference to the schemes earthing and bonding policy by others should be made.
 - All steel guard rails, gratings and supports to be galvanneal.
 - Existing hand rail and fixings to be removed and stored on site for reuse.
 - Extent and position of demolition to be approved by the Engineer on site.
 - All demolition works shall be in accordance with Appendix 26 of the Contract Specification.
 - All materials (concrete, excess reinforcement) removed during the demolition process shall be disposed off site.
 - Existing reinforcement to be cut if necessary.
 - Proposed level of weir (32.150) to be approved by the Engineer on site prior to casting in accordance with ULE9039-01-NOP-0001.

1	ISSUED FOR COMMENT	A.D.	CS	PMK	29.07.10
REV.	DESCRIPTION OF REVISION	DATE	BY	APP.	DATE

ISSUED FOR CONSTRUCTION

EDINBURGH TRAM NETWORK

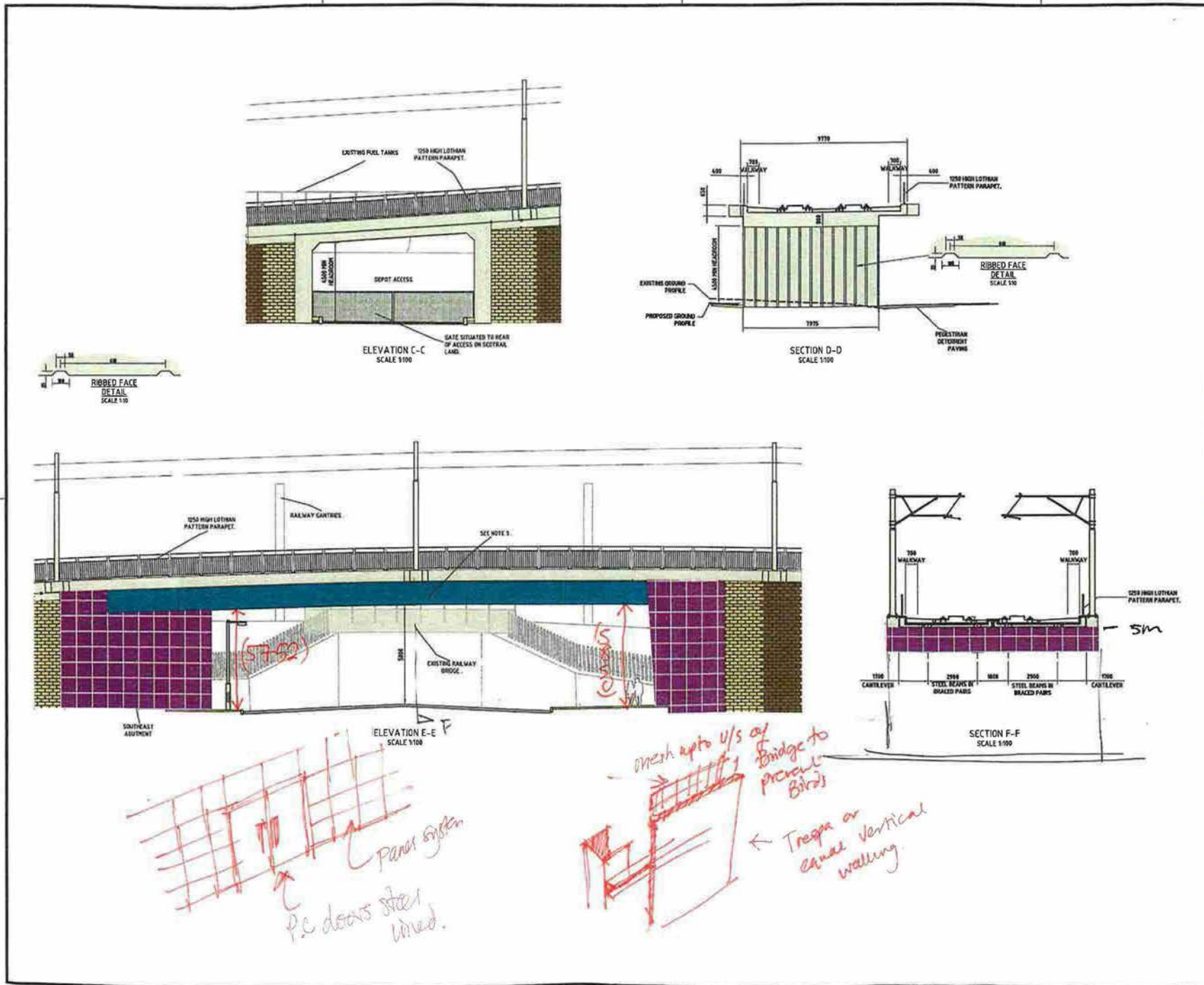
PARSONS BRINCKERHOFF

GOGAR BURN CULVERTS NOS. 1, 2 & 3 STRUCTURES 530, 531 & 534 CULVERT No. 3 OPTIMISATION

DESIGNED BY	M. CLARKE	CHECKED BY	C. SHORT
CONFERRED BY	C. WALKER	ISSUED DATE	01/08/10
DATE	30.09.07	SCALE	AS SHOWN
PROJECT NUMBER	ULE9039-07-BRG-00083	REV.	1

RESIDUAL HAZARDS	
The list shows the top residual hazards based on risk analysis. The client is to be kept in consultation with the full residual risk register (120213-01-100-0200).	
Ref. No.	Description of Risk
SR-42-01	Working in streams/vegetated areas
SR-42-02	Lifting operations
SR-42-03	Water borne disease (eg. Bath disease) Contaminated water
SR-42-04	Operation of heavy plant and machinery
SR-42-05	Pollution to watercourses from construction related activities
SR-42-06	Site vehicles using/crossing the existing road network across the site
SR-42-07	Working on steep slopes
SR-42-08	Ground conditions - waterlogged sites
SR-42-09	Transportation of excess material

CEC00218042_0054



- NOTES
1. THIS DRAWING HAS BEEN PRODUCED FOR THE FINAL APPROVAL APPLICATION AND IS NOT TO BE USED FOR CONSTRUCTION.
 2. THIS DRAWING SHALL BE READ IN CONNECTION WITH DRAWINGS ULE9016-05-PLG-00255 AND REV 1.
 3. ACCESS GATE TO BE LOCATED ON RAILWAY DEPOSIT SIDE OF STRUCTURE, BUT AT STREET.
 4. STEEL BEAMS TO BE PAINTED AS PER BS 540:541.

KEY

[Green hatched]	BRICK
[Blue hatched]	TYPICAL
[Yellow hatched]	FOOTWAY
[White]	CONCRETE



1	ISSUED FOR APPROVAL	11	10/11	10/11	10/11
2	ISSUED FOR APPROVAL	11	10/11	10/11	10/11
REV.	DESCRIPTION OF REVISION	DATE	BY	CHKD.	APP.
DRAWN BY: [Name]					

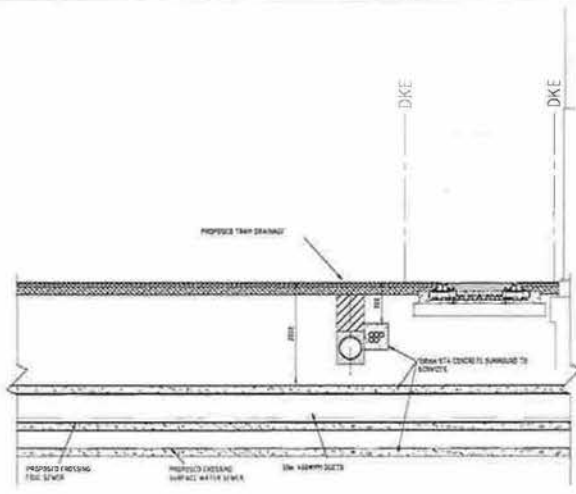
Issued for External Approval

EDINBURGH TRAM NETWORK

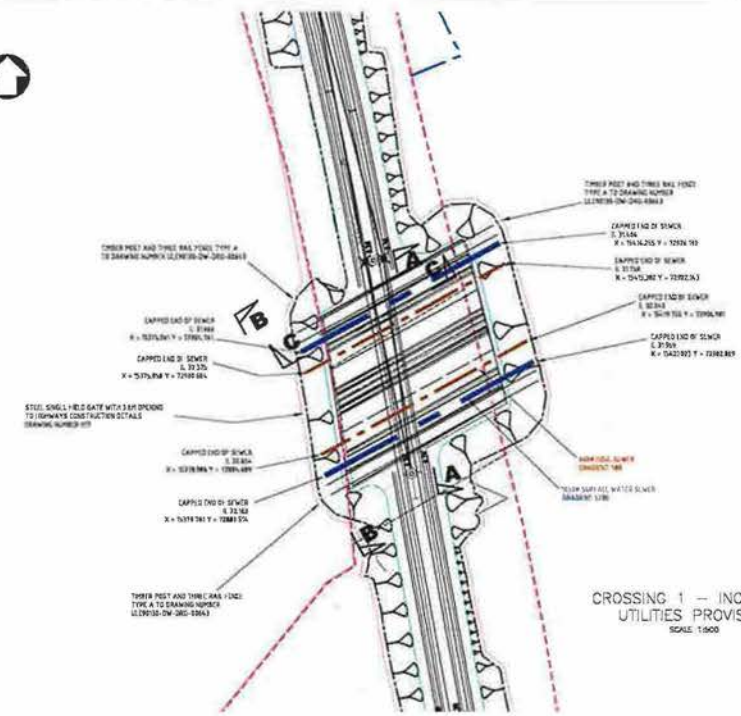
ROSEBURN ANCHOR TO GOGARBURN HURRAYFIELD & ROSEBURN VIA DUFFY PLANNING DRAWINGS BATCH 5/23 SECTIONS AND ELEVATIONS SHEET 2 OF 2

DESIGNED BY	CHKD BY	DATE	SCALE	SHEET
CHKD BY	DATE	SCALE	SHEET	

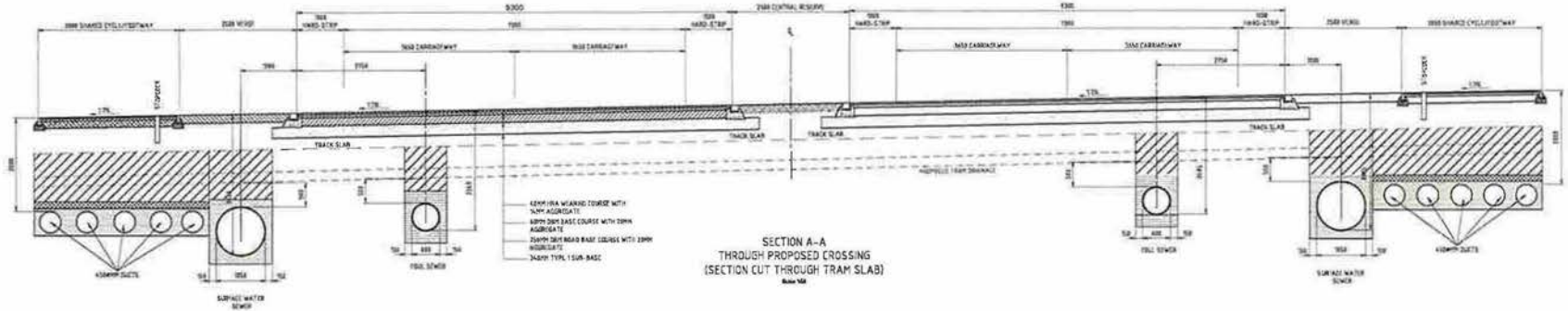
ULE9016-05-PLG-00255 2



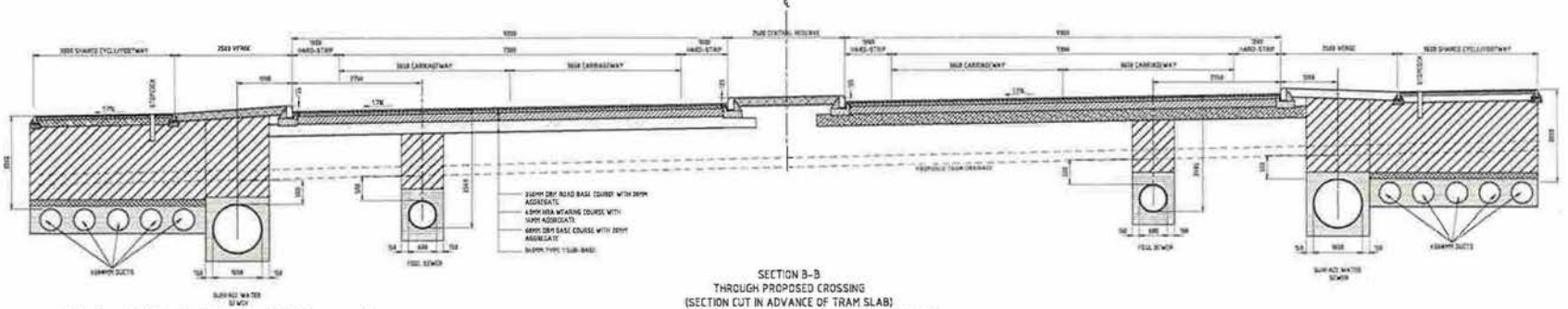
SECTION C-C THROUGH PROPOSED CROSSING
SHOWING SERVICES DUCTS
Scale 1:50



CROSSING 1 - INCLUDING UTILITIES PROVISION
SCALE: 1:500



SECTION A-A THROUGH PROPOSED CROSSING
(SECTION CUT THROUGH TRAM SLAB)
Scale 1:50



SECTION B-B THROUGH PROPOSED CROSSING
(SECTION CUT IN ADVANCE OF TRAM SLAB)
Scale 1:50

- NOTES
1. FOR CRANAGE DETAILS REFER TO DRAWING FOR SCOTLAND VIKING'S
 2. FOR FACE OF'S REFER TO DRAWING NUMBER
 3. FOR GATE DETAILS REFER TO THE SPECIFICATION FOR HIGHWAY WORK
 4. HIGHWAY CONSTRUCTION DETAILS, DRAWING NUMBER J17
 5. FOR DETAILS OF DUCTS AND DRAIN PIPES REFER TO DRAINAGE
 6. UTILITIES (DW-DRG-036) AND DRAINAGE (DW-DRG-037) DRAWINGS
 7. THE TRACK FORM'S SHOWING LOCATIONS Y REFER TO TRACK FORM'S
 8. DRAWINGS FOR DETAILS
 9. FOR DETAILS OF THE ROAD PAVEMENT REFER TO DRAWING NUMBER
 10. ALTERNATIVE DW-DRG-036-010
 11. THE DRAWING TO BE READ IN CONJUNCTION WITH DRAWING NUMBER
 12. LENGTHS: 41-DRG-036-010

LOCATION DIAGRAM - LINE 2



NO.	DESCRIPTION	DATE	BY	CHKD BY	DATE
1	ISSUED FOR EXTERNAL APPROVAL	09/23/2010	AS	AS	09/23/2010

Issued for External Approval

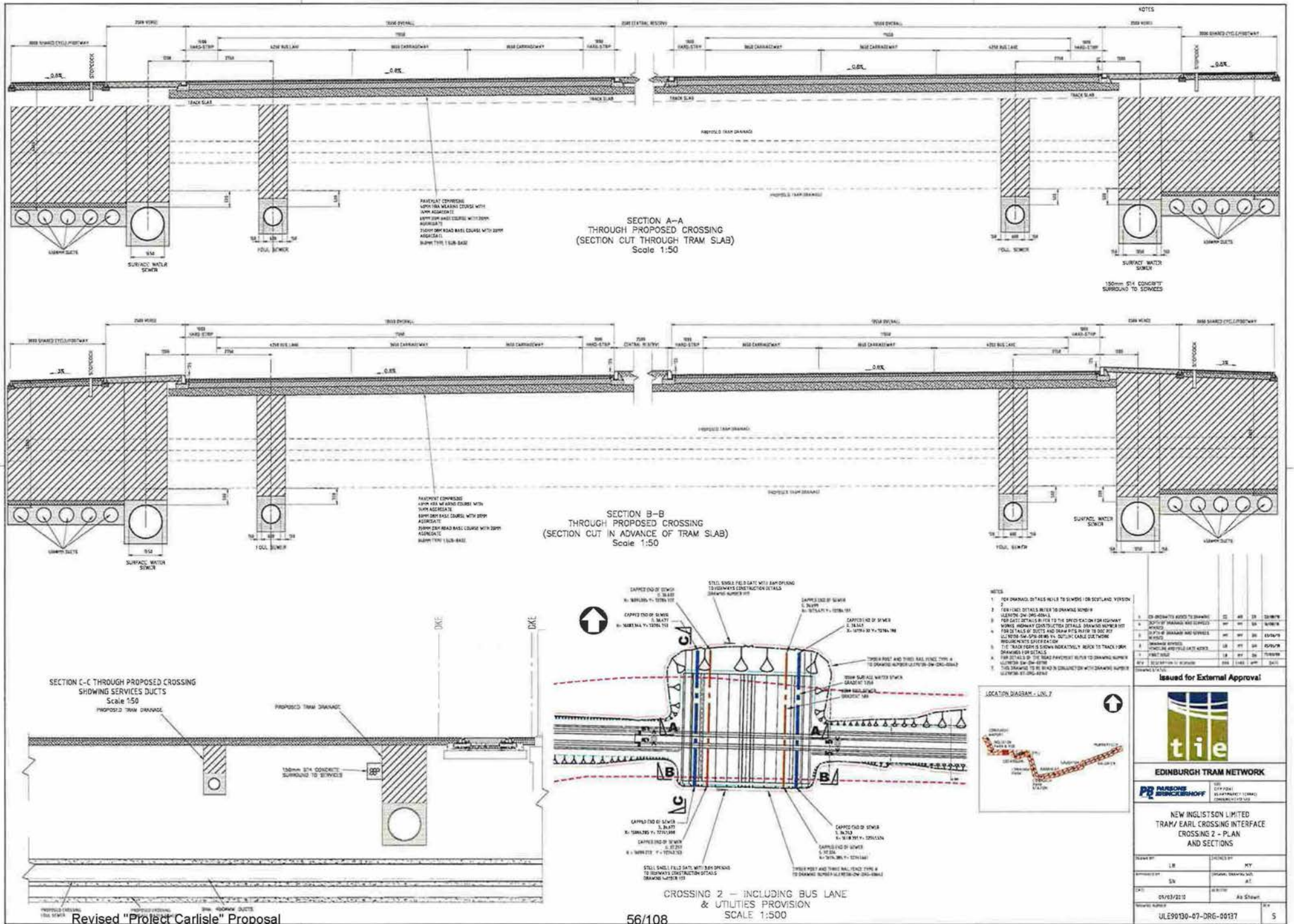
EDINBURGH TRAM NETWORK

PARSONS BRINCKERHOFF

NEW INGLISTON LIMITED
TRAM / EARL CROSSING INTERFACE
CROSSING 1 - PLAN AND SECTIONS

DESIGNED BY	AS	CHECKED BY	MY
APPROVED BY	SN	DESIGN DRAWING NO.	JL1
DATE	09/23/2010	SCALE	As Shown
PROJECT NUMBER	L1LE9130-07-DRG-00136	REV	5

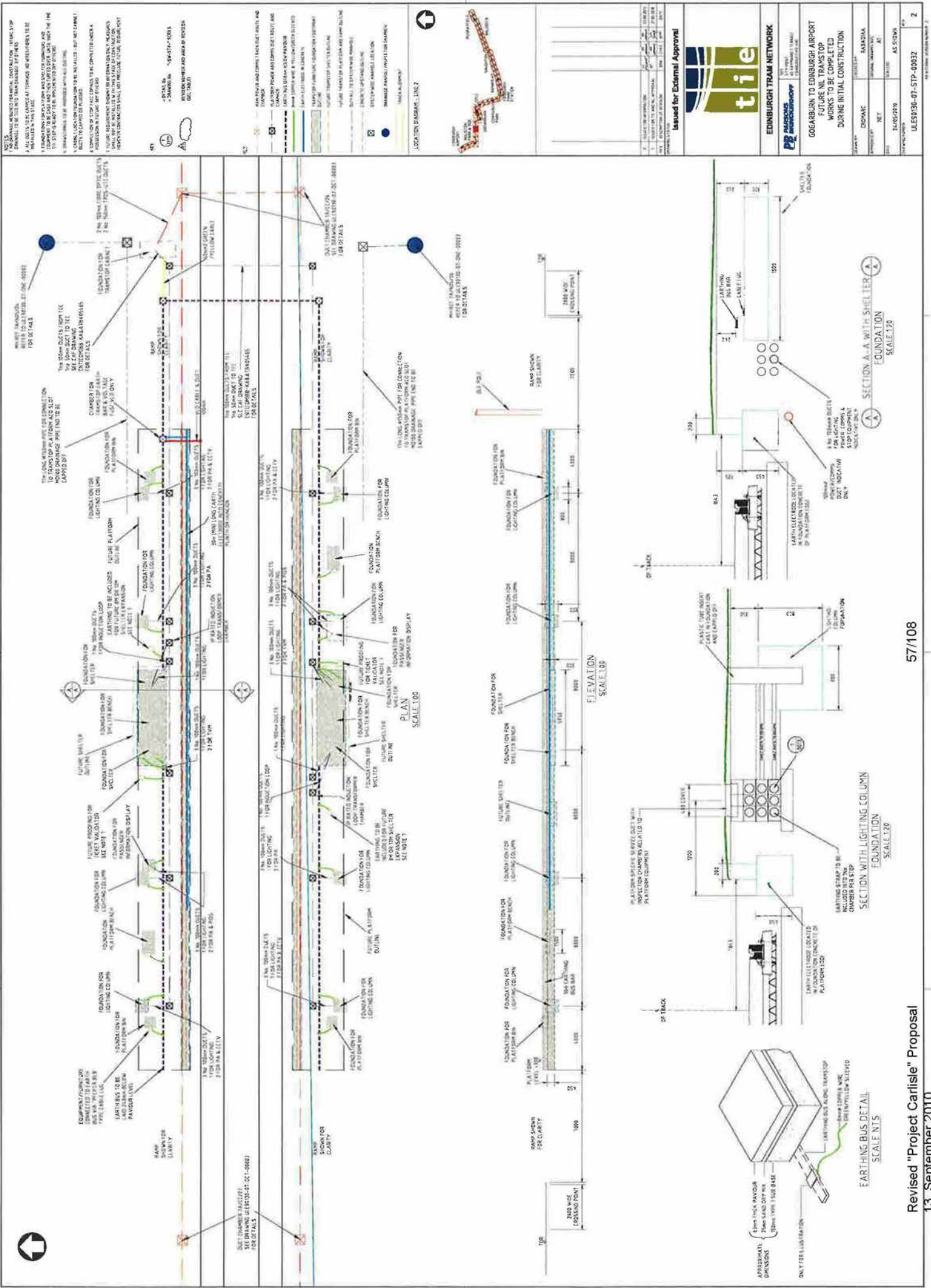
CEC00218042_0055



CEC00218042_0056

Revised "Project Carlisle" Proposal
13. September 2010

56/108



Issued for External Approval

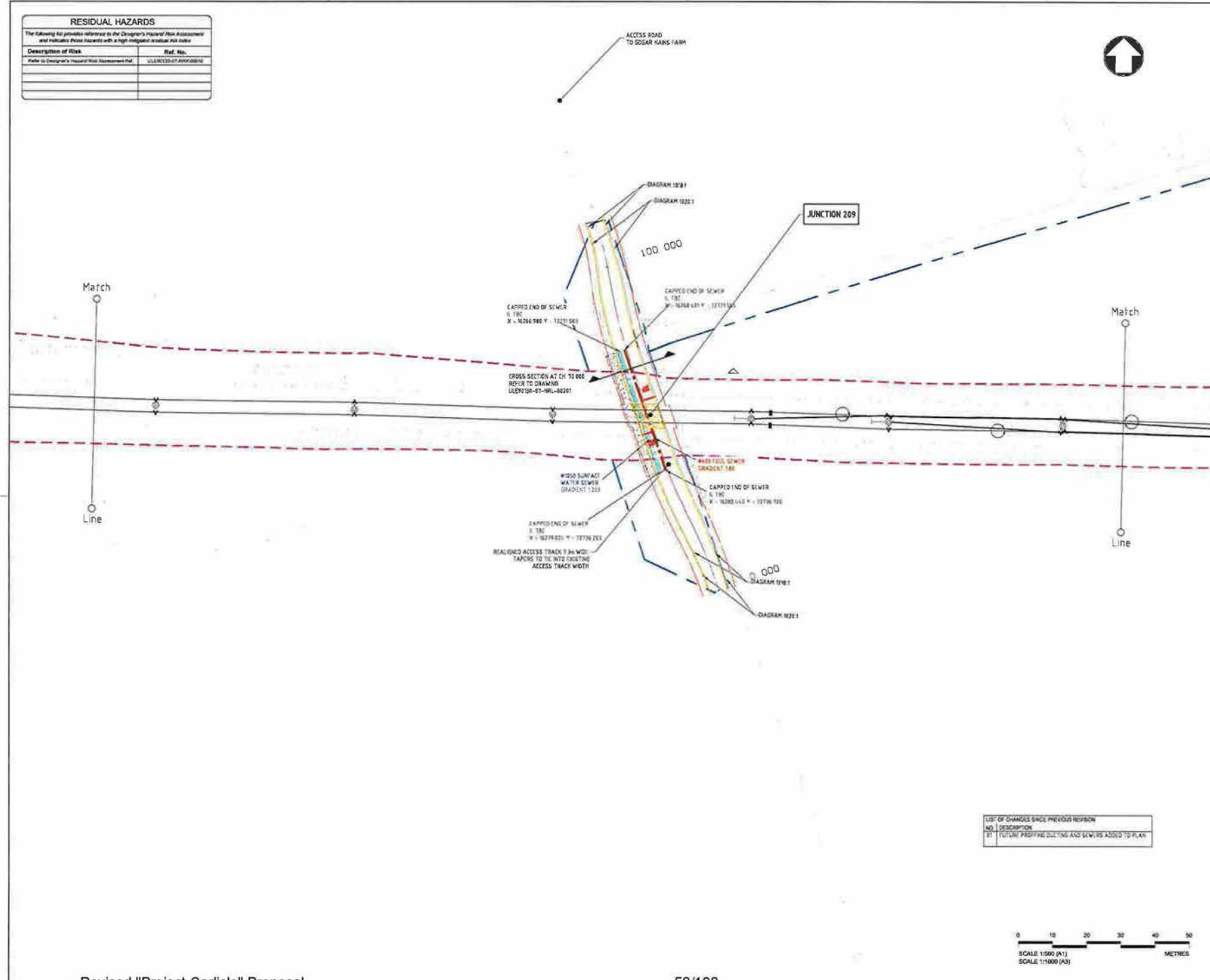
EDINBURGH TRAM NETWORK

GOGARBURN TO EDINBURGH AIRPORT
FUTURE NILE TRAM STOP
WORKS TO BE COMPLETED
DURING INITIAL CONSTRUCTION

DATE	03/04/10
BY	MARK BRIDGEMAN
CHECKED BY	MARK BRIDGEMAN
SCALE	AS SHOWN
PROJECT NO.	ULE101030-07-STP-00032



RESIDUAL HAZARDS	
The following table provides reference to the Designer's Hazard Risk Assessment and indicates those hazards with a high residual residual risk rating	
Description of Risk	Ref. No.
Refer to Designer's Hazard Risk Assessment Ref.	ULE90130-07-HRL-000010



- NOTES
- PROJECT MAPPING USED
 - DESIGN BASED ON LOCAL GRID
 - ALL DIMENSIONS IN METRES UNLESS STATED OTHERWISE
 - ALL WORKS TO BE IN ACCORDANCE WITH CEC STANDARD DETAILS
 - FOR TRAFFIC SIGNS AND ROAD MARKINGS REFER TO ULE90130-07-HRL-00003 AND 00007 TO 00009 DRAWINGS
 - FOR KERBS, FOOTWAYS AND PAVED AREAS REFER TO ULE90130-07-HRL-00003, 00007 AND 00008 DRAWINGS
 - FOR DRAINAGE LAYOUT REFER TO ULE90130-07-HRL-00010 TO 00019 DRAWINGS
 - FOR STREET LIGHTING REFER TO ULE90130-07-LIG-00007 TO 00009 DRAWINGS
 - FOR SITE CLEARANCE REFER TO ULE90130-07-SCL-00001 TO 00004 DRAWINGS
 - FOR TRAFFIC REGULATIONS ORDERS REFER TO ULE90130-07-TRO-00001 TO 00008 DRAWINGS
 - FOR LANDSCAPING DETAILS REFER TO ULE90130-07-LDS - 00001 TO 00003 DRAWINGS
 - FOR TRAM STOP DESIGN REFER TO ULE90130-07-STP SERIES DRAWINGS
 - FOR UTILITIES REFER TO ULE90130-07-UTL-00001 TO 00009 DRAWINGS

- LEGEND
- KERB LINE/CHANNEL/EDGE OF ROAD
 - EDGE OF FOOTWAY OR CYCLOWAY OR OTHER KERB
 - LIMIT OF DEVIATION (L.O.D.)
 - LIMIT OF LAND TO BE ACQUIRED OR USED
 - ELLAS
 - TRAFFIC SIGN
 - SAFETY BARRIER
 - PEDESTRIAN GUARDRAIL
 - PROPOSED TELE POLE (FOR EXACT LOCATIONS REFER TO ALL DRAWINGS)



NO.	ISSUED FOR EXTERNAL APPROVAL	CT	BR	AD	DATE
1	ISSUED FOR EXTERNAL APPROVAL	N/A	N/A	LA	25/09/10
2	ISSUED FOR EXTERNAL APPROVAL	N/A	N/A	LA	25/09/10
3	ISSUED FOR EXTERNAL APPROVAL	BT	BT	BT	25/09/10
4	ISSUED FOR EXTERNAL APPROVAL	W	CE	JMC	24/09/10
5	EXTERNAL APPROVAL	MS	ES	JA	25/09/10
6	ISSUED FOR EXTERNAL APPROVAL	MS	ES	JA	24/09/10
7	EXTERNAL APPROVAL	CE	JA	MS	25/09/10
8	ISSUED FOR EXTERNAL APPROVAL	CE	JA	MS	25/09/10
9	EXTERNAL APPROVAL	JA	MS	CE	25/09/10
10	EXTERNAL APPROVAL	JA	MS	CE	25/09/10
11	EXTERNAL APPROVAL	CE	JA	MS	25/09/10
12	EXTERNAL APPROVAL	CE	JA	MS	25/09/10

ISSUED FOR EXTERNAL APPROVAL



EDINBURGH TRAM NETWORK

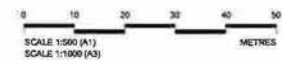


GOGARBURN TO EDINBURGH AIRPORT ROAD SCHEME LAYOUT PLANS SECTION 7A

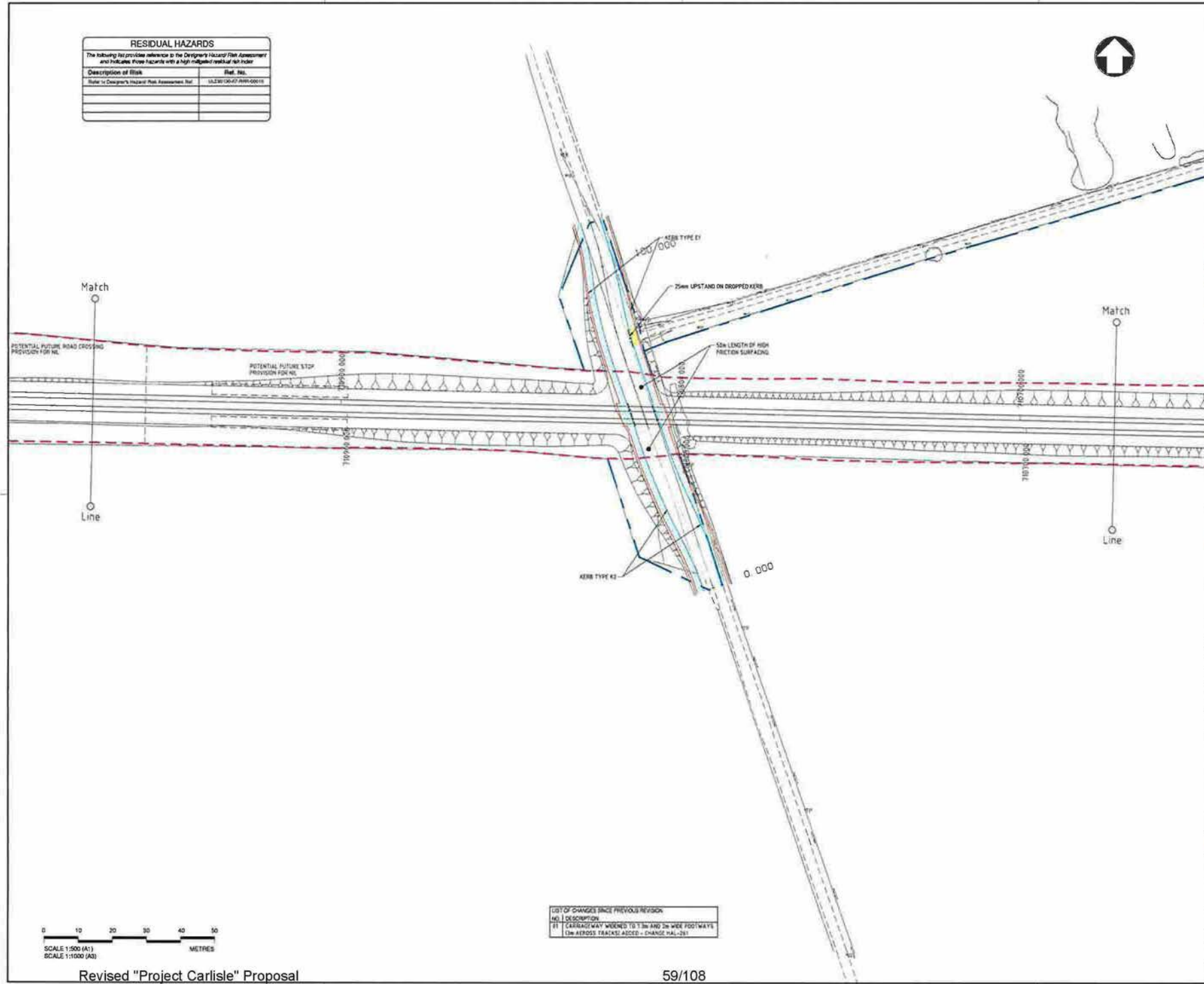
DESIGNED BY	CHECKED BY
E. RAHAY	I. ASTBURY
APPROVED BY	DESIGNED DRAWING NO.
M. BISSETT	AT
DATE	SCALE
26/05/2006	1:500
DRAWING NUMBER	NO.
ULE90130-07-HRL-00003	10

LIST OF CHANGES SINCE PREVIOUS REVISION

NO.	DESCRIPTION
01	FUTURE PAVING DUCTING AND SEWERS ADDED TO PLAN



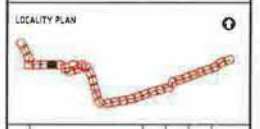
RESIDUAL HAZARDS	
The following list provides reference to the Designer's Hazard Risk Assessment and indicates those hazards with a high mitigated residual risk index	
Description of Risk	Risk No.
Refer to Designer's Hazard Risk Assessment List	06230/0047/0001-00015



- NOTES
- PROJECT MAPPING USED
 - DESIGN BASED ON LOCAL GRID
 - ALL DIMENSIONS IN METRES UNLESS STATED OTHERWISE
 - ALL WORKS TO BE IN ACCORDANCE WITH CEC STANDARD DETAILS
 - FOR KERB TYPES REFER TO DRAWING ULE90130-SM-CND-00002, ULE90130-01-HRL-01011 & ULE90130-01-HRL-01013
 - FOR DROPPED KERB DETAILS AT PEDESTRIAN CROSSINGS REFER TO DRAWING ULE90130-SM-CND-00002
 - ALL TACTILE PAVING TO BE IN ACCORDANCE WITH APPENDIX 11/1
 - FOR ROAD PAVEMENT REINSTATEMENT DETAILS FOLLOWING REMOVAL OF EXISTING SURFACE TREATMENTS REFER TO ROAD PAVEMENT CONSTRUCTION DRAWINGS ULE9193-01-HRL-00403 00407 AND 00408
 - HIGH FRICTION SURFACE TO BE BLACK COLOURED

LEGEND

[Symbol]	KERB LINE/CHANNEL EDGE OF ROAD
[Symbol]	EDGE OF FOOTWAY OR EYELEWAY
[Symbol]	DROPPED KERB
[Symbol]	LIMIT OF DEVIATION (LOD)
[Symbol]	LIMIT OF LAND TO BE ACQUIRED OR USED
[Symbol]	TRAFFIC SIGN
[Symbol]	SAFETY BARRIER
[Symbol]	PEDESTRIAN GUARDRAIL
[Symbol]	NEW FOOTWAY CONSTRUCTION BITUMINOUS SURFACING
[Symbol]	NEW FOOTWAY BITUMINOUS SURFACING
[Symbol]	EXISTING FOOTWAY TO BE BROKEN OUT
[Symbol]	REMOVE COLOURED SURFACING
[Symbol]	HIGH FRICTION SURFACING
[Symbol]	STRENGTHENED FOOTWAY SEE ITEM 18 APP 11/1 FOR VEHICLE OVERRUN



1	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
2	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
3	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
4	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
5	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
6	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
7	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
8	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
9	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
10	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
11	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
12	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
13	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
14	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
15	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
16	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
17	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
18	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
19	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10
20	ISSUED FOR EXTERNAL APPROVAL	ALA	MR	13	20/10/10

ISSUED FOR EXTERNAL APPROVAL

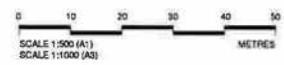


EDINBURGH TRAM NETWORK



GOGARBURN TO EDINBURGH AIRPORT
KERBS, FOOTWAYS AND PAVED AREAS
SECTION 7A

DESIGNED BY	D COOK	CHECKED BY	J ASTBURY
DATE	23/02/2007	SCALE	1:500
PROJECT NO.	ULE90130-01-HRL-00903	SHEET NO.	9

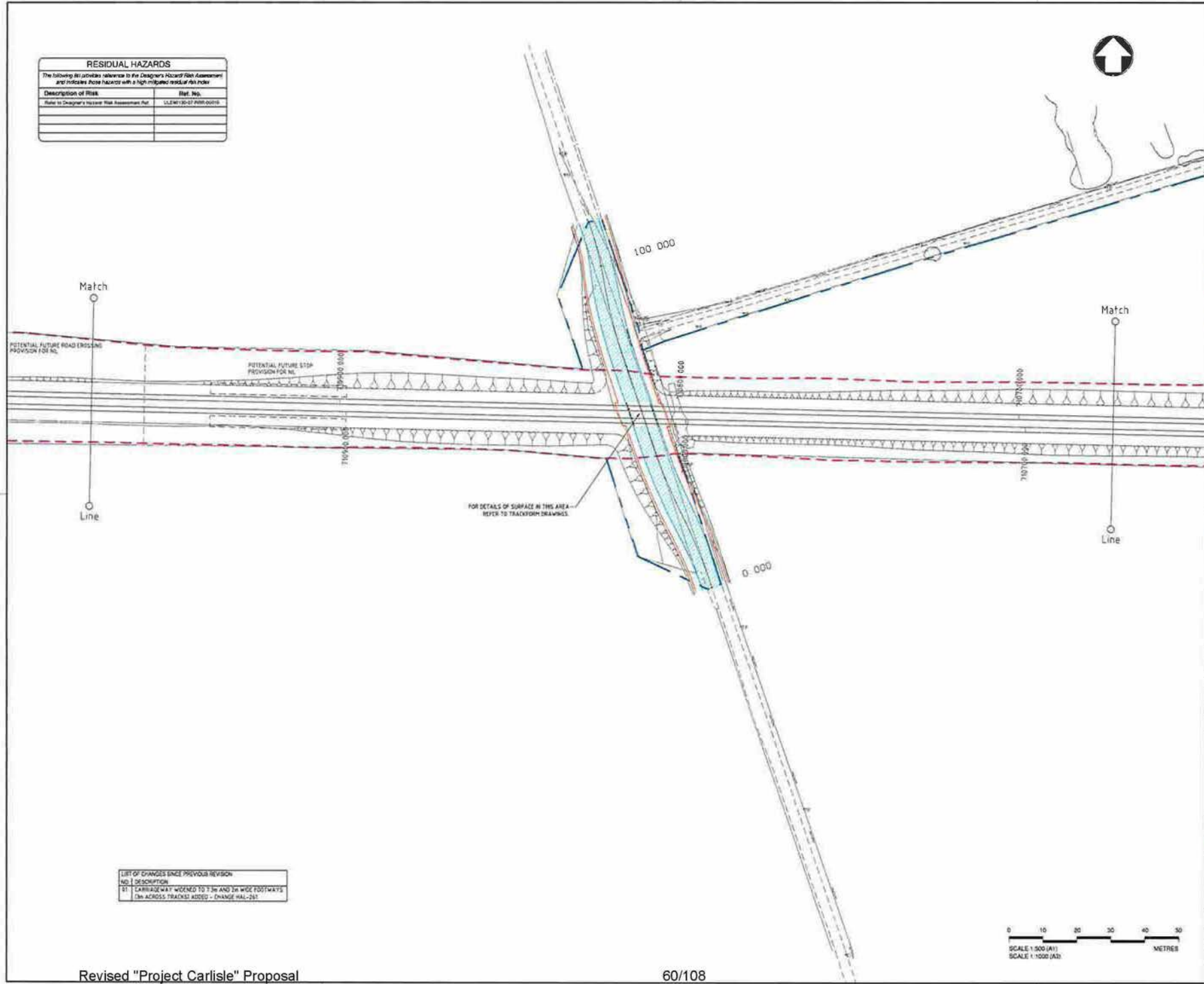


LIST OF CHANGES SINCE PREVIOUS REVISION

NO.	DESCRIPTION
01	CARBOXYMETHYL WOODEN TO 7.5m AND 2m WIDE FOOTWAYS (1m ACROSS TRACKS) ADDED - CHANGE 141-261

CEC00218042_0059

RESIDUAL HAZARDS	
The following list provides reference to the Designer's Hazard Risk Assessment and indicates those hazards with a high mitigated residual risk index	
Description of Risk	Ref. No.
Refer to Designer's Hazard Risk Assessment Act	ULE90130-07-HRL-00403



- NOTES
- PROJECT MAPPING USED
 - DESIGN BASED ON LOCAL GRID
 - ALL DIMENSIONS IN METRES UNLESS STATED OTHERWISE
 - ALL WORKS TO BE IN ACCORDANCE WITH CEC STANDARD DETAILS
 - REFER TO SPECIFICATION APPENDICES 7/1 AND 10/1 FOR PAVEMENT DESIGN OPTIONS
 - FOR TRACK SLAB AND SURFACE FINISH REFER TO TRACK SLAB DESIGN DRAWINGS
 - REFER TO SPECIFICATION APPENDIX 7/1 FOR DETAILS RELATING TO USE OF BITUMINOUS OVERLAYS
 - FOR DETAILS OF FOOTWAY CONSTRUCTION AND KERBS, REFER TO ULE90130-07-HRL-00903
 - MAJOR ROAD PAVEMENT REPAIRS ARE REQUIRED ADJACENT TO NEW KERBLINES
 - FOR DETAILS OF REINSTATEMENT REFER TO LANDSCAPING DRAWINGS ULE90130-07-LOS-00001 TO -00009

- LEGEND
- KERB LINE / CHANNEL EDGE OF ROAD
 - EDGE OF FOOTWAY OR CYCLEWAY
 - GRIPPER KERB
 - LMPT OF DEVIATION L1001
 - LMPT OF LAND TO BE ACQUIRED OR USED
 - TRAFFIC SIGN
 - SAFETY BARRIER
 - PEDESTRIAN GUARDRAIL
 - EXISTING PAVEMENT TO BE BROKEN OUT
 - EXISTING PAVEMENT TO BE BROKEN OUT AND REPLACED WITH TRACK SLAB
 - NEW ROAD CONSTRUCTION
 - BITUMINOUS SURFACING OVERLAY



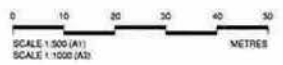
NO.	ISSUED FOR	DATE	BY	FOR
7	ISSUED FOR EXTERNAL APPROVAL	04.10.09	CEC	CEC/09/09
6	ISSUED FOR EXTERNAL APPROVAL	04.10.09	CEC	CEC/09/09
5	ISSUED FOR EXTERNAL APPROVAL	03.10.09	CEC	CEC/09/09
4	ISSUED FOR EXTERNAL APPROVAL	03.10.09	CEC	CEC/09/09
3	ISSUED FOR EXTERNAL APPROVAL	03.10.09	CEC	CEC/09/09
2	ISSUED FOR EXTERNAL APPROVAL	03.10.09	CEC	CEC/09/09
1	ISSUED FOR EXTERNAL APPROVAL	03.10.09	CEC	CEC/09/09

ISSUED FOR EXTERNAL APPROVAL



GOGARBURN TO EDINBURGH AIRPORT
ROADS CONSTRUCTION DETAILS
SECTION 7A

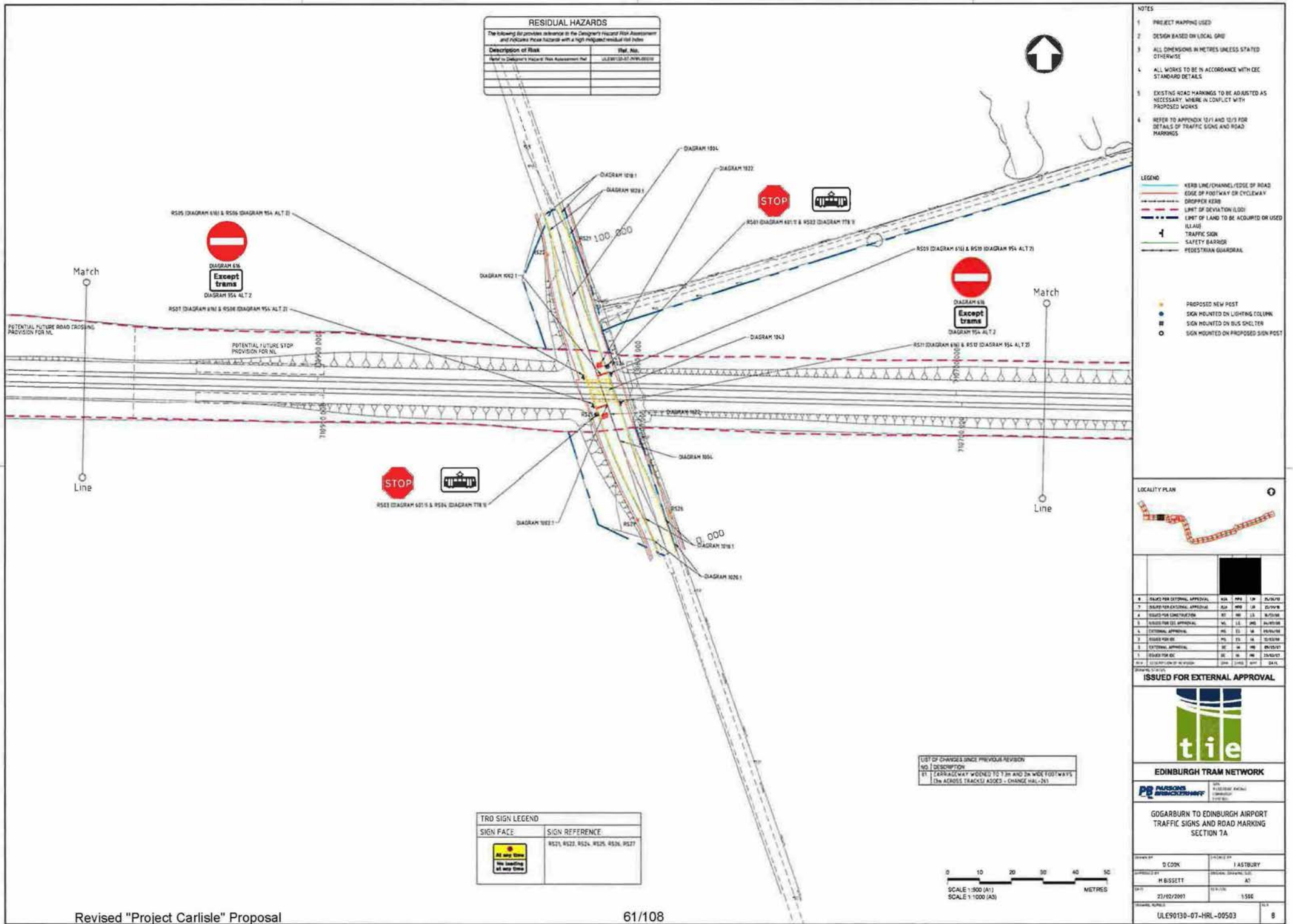
DESIGNED BY E SLODWCZUK	DRAWN BY H ROBINSON
CHECKED BY J ASTBURY	SCALE A1
DATE 21/07/2007	SCALE 1:500
PROJECT NUMBER ULE90130-07-HRL-00403	SHEET 7



LIST OF CHANGES SINCE PREVIOUS REVISION

NO.	DESCRIPTION
01	LARRIDGEWAY WIDENED TO 7.3m AND 2m WIDE FOOTWAYS (3m ACROSS TRACKS) ADDED - CHANGE HAL-267

CEC00218042_0060

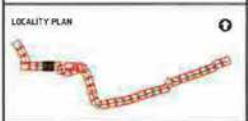


RESIDUAL HAZARDS	
The following list provides reference to the Designer's Hazard Risk Assessment and FIDIC's Hazard Risk Assessment with a high risk residual risk index	
Description of Risk	Ref. No.
Refer to Designer's Hazard Risk Assessment Ref	ULE90130-07-NRPL0010

- NOTES
- PROJECT MAPPING USED
 - DESIGN BASED ON LOCAL GRID
 - ALL DIMENSIONS IN METRES UNLESS STATED OTHERWISE
 - ALL WORKS TO BE IN ACCORDANCE WITH DEC STANDARD DETAILS
 - EXISTING ROAD MARKINGS TO BE ADJUSTED AS NECESSARY, WHERE IN CONFLICT WITH PROPOSED WORKS
 - REFER TO APPENDIX 10/1 AND 10/3 FOR DETAILS OF TRAFFIC SIGNS AND ROAD MARKINGS

- LEGEND
- HERB LINE/CHANNEL/EDGE OF ROAD
 - EDGE OF FOOTWAY OR CYCLEWAY
 - DROPPER KERB
 - LIMIT OF DEVIATION (LOD)
 - LIMIT OF LAND TO BE ACQUIRED OR USED (LLAU)
 - TRAFFIC SIGN
 - SAFETY BARRIER
 - PEDESTRIAN GUARDRAIL

- PROPOSED NEW POST
- SIGN MOUNTED ON LIGHTING COLUMN
- SIGN MOUNTED ON BUS SHELTER
- SIGN MOUNTED ON PROPOSED SIGN POST



8	ISSUED FOR EXTERNAL APPROVAL	8/11	8/11	8/11	8/11/2010
7	ISSUED FOR EXTERNAL APPROVAL	8/11	8/11	8/11	8/11/2010
6	ISSUED FOR EXTERNAL APPROVAL	8/11	8/11	8/11	8/11/2010
5	ISSUED FOR EXTERNAL APPROVAL	8/11	8/11	8/11	8/11/2010
4	EXTERNAL APPROVAL	8/11	8/11	8/11	8/11/2010
3	ISSUED FOR EXTERNAL APPROVAL	8/11	8/11	8/11	8/11/2010
2	EXTERNAL APPROVAL	8/11	8/11	8/11	8/11/2010
1	EXTERNAL APPROVAL	8/11	8/11	8/11	8/11/2010
0	ISSUED FOR EXTERNAL APPROVAL	8/11	8/11	8/11	8/11/2010

ISSUED FOR EXTERNAL APPROVAL



EDINBURGH TRAM NETWORK



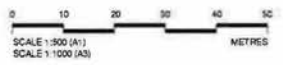
GOGARBURN TO EDINBURGH AIRPORT
TRAFFIC SIGNS AND ROAD MARKING
SECTION 7A

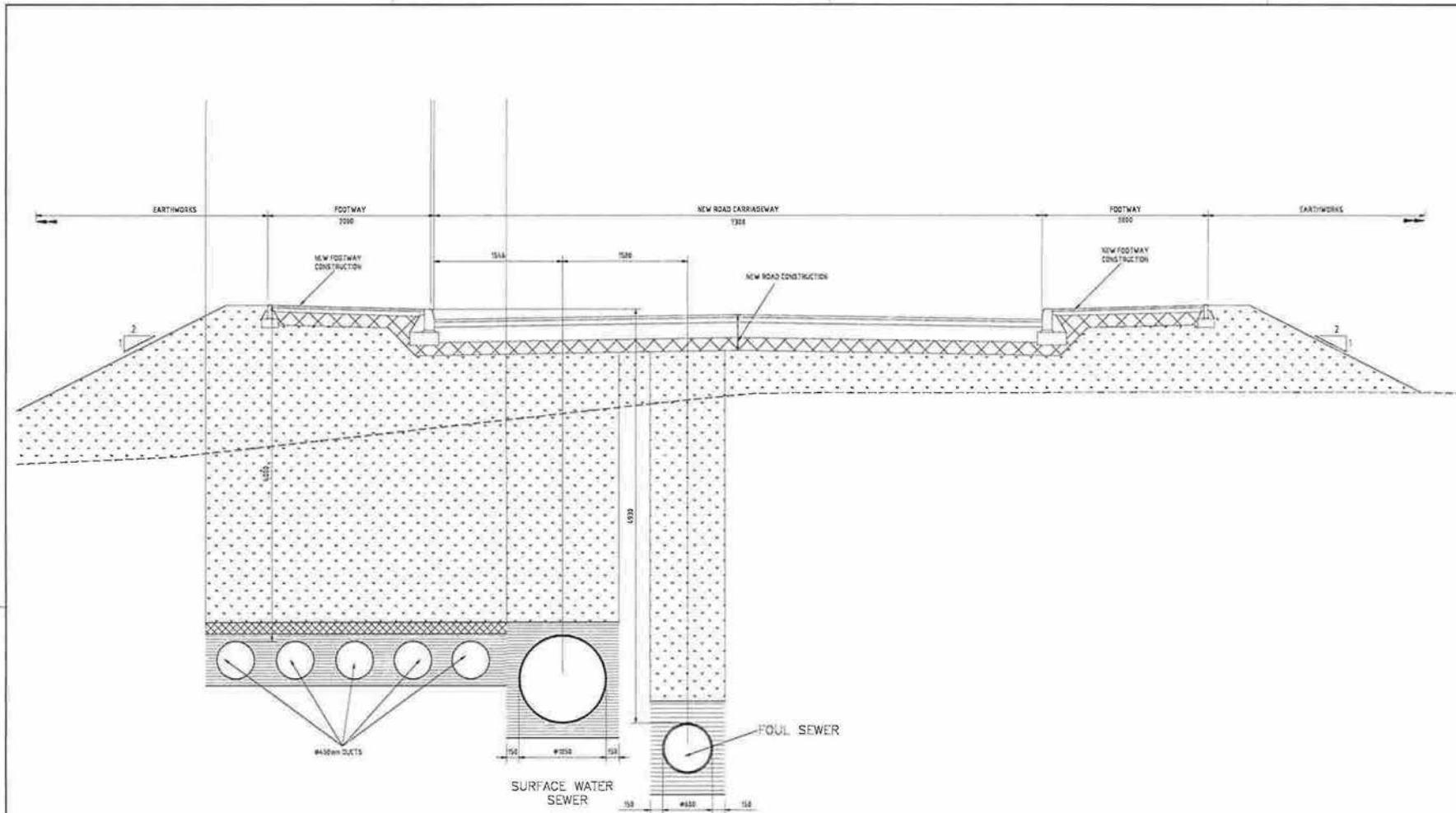
DESIGNED BY	D COOK	CHECKED BY	J ASTBURY
APPROVED BY	M BISSETT	DESIGNED BY (LOCAL)	AD
DATE	27/02/2010	SCALE	1:500
DRAWING NO.	ULE90130-07-HRL-00503	SHEET NO.	8

TRO SIGN LEGEND	
SIGN FACE	SIGN REFERENCE
	RS21, RS22, RS24, RS25, RS26, RS27

LIST OF CHANGES SINCE PREVIOUS REVISION

NO.	DESCRIPTION
01	CARRIAGEWAY WIDENED TO 7.5m AND 2m WIDE FOOTWAYS (1.5m ACROSS TRACKS) ADDED - CHANGE HAL-241





TYPICAL ROAD CROSS SECTION
PERPENDICULAR TO GOGAR MAINS FARM ACCESS ROAD

- NOTES
1. ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE STATED
 2. ALL WORKS TO BE IN ACCORDANCE WITH CEC STANDARD DETAILS UNLESS STATED OTHERWISE

- LEGEND
- EXISTING GROUND LEVEL
 - SURFACING
 - ROAD BASE
 - SUB-BASE
 - CAPPING
 - FOOTWAY/FOOTPATH SURFACING
 - FOOTWAY/FOOTPATH CONSTRUCTION
 - EARTHWORKS

1	ISSUED FOR EXTERNAL APPROVAL	21	08	10	21/08/10
2	ISSUED FOR EXTERNAL APPROVAL	24	08	10	24/08/10
3	ISSUED FOR EXTERNAL APPROVAL	24	08	10	24/08/10
4	ISSUED FOR CONSTRUCTION	25	08	10	25/08/10
5	ISSUED FOR EXTERNAL APPROVAL	25	08	10	25/08/10
6	EXTERNAL APPROVAL	25	08	10	25/08/10
7	ISSUED FOR EXTERNAL APPROVAL	25	08	10	25/08/10
8	EXTERNAL APPROVAL	25	08	10	25/08/10
9	ISSUED FOR EXTERNAL APPROVAL	25	08	10	25/08/10
10	ISSUED FOR EXTERNAL APPROVAL	25	08	10	25/08/10

ISSUED FOR EXTERNAL APPROVAL



EDINBURGH TRAM NETWORK



TYPICAL CROSS SECTION
SECTION 7A
GOGAR MAINS FARM ACCESS ROAD

DESIGNED BY	D. COOK	CHECKED BY	J. ASTBURY
APPROVED BY	H. BISSSETT	ORIGINAL APPROVED BY	A1
DATE	23/02/07	SCALE	1:20
WORKING NUMBER	ULE90130-07-HRL-00201		

NO.	DESCRIPTION
1	SECTION 7A ADDED TO CROSS SECTION

RESIDUAL HAZARDS	
The following list provides reference to the Designer's Hazard Risk Assessment and indicates those hazards with a high mitigated residual risk index.	
Description of Risk	Risk No.
Water in Designer's Hazard Risk Assessment that	CALLS FOR ATTENTION

APPENDIX 4

PROJECT CARLISLE SCOPE

As previously stated, we have updated this Revised Proposal to reflect the scope Infraco will be able to agree on. In essence, we will carry out the works shown on the GMP Drawings for scope of work between the Airport to Haymarket, and the enabling works.

In General

The Guaranteed Maximum Price is for the construction and delivery of the design shown on the drawings, specifications, schedules and submissions contained in the electronic record of "GMP Drawings", as they pertain to the scope of work. Changes in Procurement Laws, Tax Rates, Design Standards or Specifications, or changes in the Code of Construction Practice shall be considered a tie Change.

For the avoidance of doubt and further reference below to the Guaranteed Maximum Price being based on the GMP Drawings in the individual localities, is simply for added emphasis. Infraco shall not be obliged to build anything other than the design shown in the GMP Drawings unless instructed otherwise by tie through a tie Change Order, or as negotiated between the Parties.

Specific Identification of the Scope

1. **HAYMARKET TO AIRPORT** (including Enabling Works)
 - 1.1.1 Haymarket Corridor (chainages 200000 to 200814)
 - 1.1.2 Roseburn Junction to Balgreen (chainages 510000 to 511477);
 - 1.1.3 Balgreen to Edinburgh Park Central (chainages 520000 to 524555);
 - 1.1.4 Edinburgh Park Central to Gogar (chainages 530000 to 531898);
 - 1.1.5 Gogar Depot;
 - 1.1.6 Gogar to Edinburgh Airport (chainages 710000 to 712580).
- 1.2 All enabling works as shown in the sketches included in Infraco's Project Carlisle Proposal, dated 29 July 2010:
- 1.3 The completed, approved, assured, integrated and compliant Design for the Edinburgh Tram Network Phases 1a, to include certification thereof by each Infraco Member, the SDS Provider and any Infraco Party or SDS Provider Party involved in the production or development of Design or the Infraco's Design.

Exclusions and Qualifications

Roseburn Viaduct

Pricing is based as defined in the electronic record of "GMP Drawings" which are the IFEA Drawings (issued for external Approval) issued to CEC and Scottish Power for approval. Any impact on the design of the "GMP Drawings" shall be dealt with separately as a tie Change and an appropriate tie Change Order will be issued.

Pricing is based on the basis of an inclusion within GMP, the installation of a suitable and secure gate at the entrance to the portal (S502) to prevent unwarranted access to the Haymarket Rail Depot and potential for graffiti within the portal structure and on the basis of an inclusion within GMP, the installation of a "Trespa" system or similar approved architectural lightweight cladding finish on the face of the street facing walls of S501, S503 and S504 as an additional anti-graffiti measure and to improve the amenity of the area, a small area of additional soft landscaping up to a maximum area of 30m², and the installation of false walls in front of the abutments of S504 based on the "Trespa" system or similar to match the cladding of the street facing walls. Initial details are shown on Drawing Number ULE90130-05-PLG-00265 rev 2 draft *[based on attached sketch, to be further developed]* (See Appendix 3).

Gogarburn Landfill Site (Section 7A)

The Guaranteed Maximum Price is based on tie's recommendation, which is included in the electronic record of "GMP Drawings", which amends trackform to ballast and includes a track retaining structure in the vicinity of the 50m radius curve to the east of Gogarburn Bridge and an element of earth retention. The GMP Programme assumes that a maximum three month surcharge period is required and that access will thereafter be available to Infracore in accordance with its GMP Programme.

Outstanding Approvals etc.

Consents from Third Parties, in so far as it is required to obtain agreement of approval, have been identified when possible in the Programme. In the event other Consents or Third Party Approvals are required they shall be procured by tie in accordance with the GMP Programme, and in the event of a delay or additional cost Infracore shall be compensated by tie Change Order.

Trackform Type Confirmation

The trackform type at each location is based on the actual design as defined in the electronic record of "GMP Drawings"

Murrayfield Stadium Accommodation Works

These works are being carried out by others with an expected completion date of 31 October 2010. The GMP Programme is based upon this completion date and assumes that access will be available thereafter in accordance with the GMP Programme.

South Gyle Access Bridge – sewer clash at [Structure Number 26]

The sewer is being diverted by others with an expected completion date of 31 October 2010. The GMP Programme is based upon this completion date and assumes that access will be available thereafter in accordance with the GMP Programme.

The Guaranteed Maximum Price is based on the Drawings as included in the electronic record of “GMP Drawings”.

Gogar Interchange

The Guaranteed Maximum Price does not include for the Gogar Interchange. The Guaranteed Maximum Price is based on the GMP Drawings.

New Ingliston Limited ('NIL')

The Guaranteed Maximum Price is based on the Drawings to be included in the electronic record of “GMP Drawings”, and the drawings attached (See Appendix 3)

Airport Retaining Walls W14 & W15

The Guaranteed Maximum Price is based on an alternative piling solution to that shown in the electronic record of “GMP Drawings”. As such, the fully developed drawings will not constitute a tie Change, to the extent of the revisions to the alternative piling solution. However, in the event that an alternative piling solution cannot be achieved, the return to the original piling solution will constitute a tie Change. Any changes required to accommodate the outcome of the flood modelling works are not at the risk of Infracore save for alterations required to the weir within Culvert No.3, as indicated on the drawing attached (See Appendix 3).

Airport Kiosk Design

No further future proofing is allowed in the Guaranteed Maximum Price.

The Guaranteed Maximum Price is based on the current developed elliptical design which is included on the drawings as included in the electronic record of “GMP Drawings”.

Tramstop Design

The Guaranteed Maximum Price is based on the Drawings as included in the electronic record of “GMP Drawings”.

Substation Design

The Guaranteed Maximum Price is based on the Drawings as included in the electronic record of “GMP Drawings”.

Protection of Services

The protection and support of the gas main along Russell Road, as well any unknown service, is excluded from the Guaranteed Maximum Price. The protection and support of known services have been included in the GMP. The known services are:

INTC	Area and Description of Works
51	TNC002 : Utilities diversions & associated works - Section 5a.
52	TNC003 : Utilities diversions & associated works -Section 5b.
53	TNC004 : Utilities diversions & associated works - Section 5c. (excl 5C/PW/D/003)
53a	Section 5C - SW Diversion 5C/PW/D/003 at Gogar Roundabout
267	Carry out Scottish Water Diversion at Gogar Roundabout
268	Carry out Scottish Power Diversion at Gogar Roundabout
302	Protection of live utilities at Russell Road Bridge during construction
340	Edinburgh Park Bridge South Abutment Base - Existing service (cable)
361	Scottish Power Utility Diversion Murrayfield Underpass
369	Temp works for Thus and C & W and dismantle water main at A8 Underpass
431	Slew existing BT duct at A8 underpass
474	Additional manhole requirement - Section 5B
475	A8 Underpass - BT works - Slew 2 way and 4 way ducts
479	Proposed sewer lining at Water of Leith Bridge
517	Scottish Power Utility Diversion 5A/SP/D/05 at Russell Road Retaining Wall
523	A8 Underpass - support and protect BT equipment
555	Section 5B - Busgate, protection / diversion of existing services
559	5C A8 Underpass - SGN Diversion
560	5C - A8 Underpass - Grouting of existing sewer
561	5C - SP Cable at Gyle Stop Retaining Wall (Conflict ref 5C/SP/11)
577	Section 5C - W19 Gyle Stop RTW - Protection of existing services
601	5B - Busgate - SP LV Cable
602	5B - Busgate - Scottish Water existing Sewer
604	5B - Busgate - Street lighting power supply cable and ducting
611	5B - Busgate - Existing drainage
616	A8 Underpass - slew & protect GEONET apparatus
635	2A Haymarket Viaduct - Existing BT cable and drainage pipe

Landscaping

In respect of landscaping works, Infracore shall be deemed to have met all requirements relevant to landscaping which are necessary to enable the issue of a Certificate of Sectional Completion provided that the only outstanding works relate to planting. Infracore shall then carry out the planting works at the beginning of the next planting season.

Section 2A at Haymarket: Phase 1a/1b Interface Design

The Guaranteed Maximum Price is based on the Drawings as included in the electronic record of "GMP Drawings". The Guaranteed Maximum Price does not include any work within Section 3A as part of Phase 1a or Phase 1b.

Network Rail: Form C Submissions

The Guaranteed Maximum Price is based on the standard requirements for Form C submissions and on approvals being given in line with the Programme. tie shall assist the Infraco in obtaining all relevant approvals.

Third Party Agreements at Edinburgh Park and Airport

The timetable constraints as detailed in the Third Party Agreement licence with Edinburgh Airport Ltd. (EAL), other than agreeing respective completion dates for the approved packages are not applicable in executing the scope of Project Carlisle.

The existing New Edinburgh Ltd. (NEL) license has deadlines of 18 months (excluding Edinburgh Park Viaduct) and 24 months (at the viaduct), which are about to expire. For the purposes of the Guaranteed Maximum Price Infraco has assumed that the required timeframes can be agreed with NEL by tie / CEC to allow Infraco to construct, test and commission in accordance with the GMP Programme.

Any impact of these dates on the GMP Programme shall be dealt with separately as a tie Change and an appropriate tie Change Order will be issued.

Overall Traffic Management Model Approval

The Guaranteed Maximum Price is based on the drawings, specifications and schedules ("GMP Drawings"). The Infraco shall comply with the requirements of the Employer's Requirements. Any required Change to the Employer's Requirements or any preferential or other design changes required by CEC will be dealt with separately as a tie Change and an appropriate tie Change Order will be issued.

OLE Vehicle Impact Report

Infraco is responsible for obtaining CEC's acceptance and agreement provided such acceptance and agreement is provided in accordance with the timescales assumed in the programme. To date, CEC has not responded within the Approval Period as specified within the Infraco Contract. Any required Change due to late comments by CEC will be dealt with separately as a tie Change and an appropriate tie Change Order will be issued.

Lochside Avenue Junction

The Guaranteed Maximum Price is based on the drawings, specifications and schedules approved by CEC on 20 May 2010 and included in the electronic record of "GMP Drawings". The Guaranteed Maximum Price assumes no further comments or Informatives raised by CEC beyond those comments provided with the Approval of this design.

Changes to Traffic Regulation Orders (TROs)

tie is responsible for promotion and making of all necessary TROs to meet the PC Programme. tie confirms that Infraco have provided all necessary documentation and support required to support TRO 1 (defined as necessary for tram operations) for all on-street (Line 1) and off street (Line 2) sections. Further, Infraco is not responsible for the cost of any changes or amendments to any TRO documentation (drawings, schedules or reports) or to the design as a result of the TRO process or for any delays to the PC Programme due to a delay in the TRO process. Infraco is not responsible for the cost of any abortive works or delays due to the final "made" TRO impacting on the IFC design or changes to the GMP Drawings.

Drainage Alignment Drawings (Off-Street)

Any changes required to the current design on the drawings, specifications and schedules ("GMP Drawings), including both the impact on design and in construction, shall be dealt with separately as a tie Change and an appropriate tie Change Order will be issued.

OLE Interface at Tower Place Bridge

The Guaranteed Maximum Price is based on the drawings, specifications and schedules ("GMP Drawings").

OLE Interface at Russell Road Retaining Walls 3 & 4

The Guaranteed Maximum Price is based on the drawings, specifications and schedules ("GMP Drawings").

Interfaces with Energy Suppliers at Depot

Agreement for Local Power supply, meter arrangements, and gas supply, in this area remain outstanding. Additional issues regarding Way-leave clarification are also outstanding. Any further delay in these matters incurred beyond 12 July 2010 shall be to the full responsibility of tie, and subject to an extension of time.

Washing Plant

The Guaranteed Maximum Price is based on the Siemens proposal and the actual status of design. Any additional arrangements shall be subject to a tie Notice of Change and an appropriate tie Change Order will be issued.

Combined Poles

The Guaranteed Maximum Price includes in the material and equipment the complete set of pole extensions required to allow for the implementation of the street lighting integrated with the OLE poles for the original scope of work as indicated on the GMP Drawings.

Cable ducts

Cable duct connections between ETN and the Urban Traffic Control (UTC) and the City in View CCTV Control in a size of 150 mm diameter are not allowed in the Guaranteed Maximum Price due to an outstanding clarification on the right of way and the related design. For the execution of this work a tie change order may be required.

Noise & Vibration

Due to outstanding issues, the design and construction of the Floating Slab (Floating Track) as a Noise & Vibration protecting measures, no allowance has been made in the Guaranteed Maximum Price and a tie change order may be required.

HV / LV Supplies

The Guaranteed Maximum Price includes the value confirmed in TNC 014. (INF CORR 2304/RB). Any modification required for the HV / LV supply shall be subject to additional tie change order.

Utilities / Utility Works

The Guaranteed Maximum Price does not include for any Utilities, utilities or any Utilities Works that will be required to accommodate the Infraco Works other than those that have already been implemented through a tie Change Order.

Contaminated Materials

The Guaranteed Maximum Price includes for dealing with contamination, in the locations listed below, to a maximum aggregate quantity of 51,000 cubic metres, comprised of 13,000 cubic metres of hazardous material, and the remaining 38,000 considered non-hazardous. Any additional amounts shall be addressed through a tie Change Order.

2A Haymarket Yards Replacement Carpark
2A Russell Road RWs W3 & W4 including temporary stockpile on Haymarket Yards Car Park
W18 Retaining Wall at Murrayfield Tramstop
S21B and D Murrayfield Stadium and Training Pitches Retaining Walls
W8 Baird Drive Retaining Wall
S23 Carricknowe Bridge South Approach Ramp
5B Edinburgh Park Bridge to South Gyle Access Bridge

Below ground obstructions/voids

The Guaranteed Maximum Price does not include for any occurrence of archaeological findings. All obstructions and voids shall be addressed through a tie Change Order.

Duct Bank to City Chambers

The GMP does not include for duct construction from Princes Street to the City Chambers.

Design Assurance Statements

Infraco will carry out all necessary assurance statements, approvals and consents for the design, construction, testing and commissioning of the Edinburgh Tram Network as defined in this Revised Proposal (Phase 1a), with respect to the Safety Assurance Plan to enable tie to comply with ROGS and other key statutory requirements.

Ongoing tie Change Orders

Infraco and tie are in the process of negotiating numerous tie Change Orders, however it is unlikely and unnecessary that they will be finalised prior to submission of this Revised Proposal, in so far as the estimated amounts of these tie Change Orders have been incorporated in the GMP. Since Change Orders may have been issued after 30 June 2010 the GMP will require minor adjustment, as that the GMP only makes allowances for tie Change Orders issued prior to this date. However, in the instances where tie has issued Change Orders which contain qualifications, the qualifications remain valid and have been utilised in the development of the GMP.

Siemens' Materials and Equipment regarding the sections east of Haymarket

The Guaranteed Maximum Price includes the value of all materials and equipment which Siemens has already contracted for the scope east of Haymarket. The Guaranteed Maximum Price includes for delivery of all said materials and equipment, DDP (incoterms 2000) up to the defined projects warehouse in Edinburgh area. Arrangements have to be made by tie for taking over the project warehouse after completion of construction works for the initial phase or the transport of materials and equipment to other suitable locations.

An itemised and priced material schedule will be provided for the scope of work between Haymarket and Newhaven.

The Guaranteed Maximum Price is based on Infraco providing irrevocable vested ownership of these materials and equipment in favour of CEC, including transfer of title, risk and custody at a date which shall not exceed the Section C, Sectional Completion Date identified on the Programme, Revision 1- refer to activity ID 1000 of the Programme. For clarity, this is the latest date at which the last material or equipment should have been incorporated into the Works under the Programme, Revision 1.

The Guaranteed Maximum Price is predicated on the "Project Carlisle Change order allowing the immediate payment, per the itemised material schedule and within 60 days after physical delivery to the projects warehouse, of each and every such material and equipment for the scope of work between Haymarket and Newhaven.

The Guaranteed Maximum Price includes for warranty for all the above materials and equipment, for a warranty period expiring on 10 March 2013, i.e. two years after the Section C, Sectional Completion Date identified on the Programme, Revision 1.

Due to missing timeframes, cost for storage of the materials and equipment after providing irrevocable vested ownership as defined above is excluded from the Guaranteed Maximum Price. Siemens do not have long term arrangements beyond the Section C, Sectional Completion Date identified on the Programme; Revision 1 for its outsourced storage facilities in the Edinburgh area, the Guaranteed Maximum Price only includes cost for storage of the materials and equipment at Siemens' own or subcontracted onshore and/or offshore facilities, for the above, until that date.

Tram Supply:

The Guaranteed Maximum Price includes for all costs incurred by CAF in storing trams at their factory in Spain, and all other related costs (insurance, warranties etc) in accordance with Project Carlisle (See Appendix 1.3). For the sake of clarity, any reduction in the number of trams is not accepted by Infraco.

Designs Services (SDS)

Included in the Guaranteed Maximum Price is the cost for the preparation of all known works that the SDS Provider is currently aware of to complete the current scope. This includes the outstanding works relating to the original SDS design, design that has been subject to changes due to the alignment workshop process, changes requested by Infracore and changes driven by revised requirements from third party stakeholders and approvals bodies such as City of Edinburgh Council, tie, BAA, Forth Ports etc.

Also included in Appendix 1.4 CD, is a schedule of potential tie Changes that have been initiated, but for which, at the time of preparation of the Guaranteed Maximum Price, the scope was undefined. In these areas no instruction has been issued by tie. In most cases these tie Changes impact on a design that is complete, and compliant with the Employer's Requirements and the requirements of the Tram Design Manual, Third Party Agreements, CEC Standards and other core reference materials. These changes, if instructed, will result in changes to the GMP Drawings.

For the sake of clarity, these tie Changes have been excluded from the Guaranteed Maximum Price scope as it is recognised that it is entirely within the gift of tie to choose not to amend the current design presented and thereby mitigating cost and programme impact. Having reviewed the potential tie Changes for cost and programme savings we can see no added benefit in adopting the changes proposed in this schedule.

In preparing the Guaranteed Maximum Price, SDS has assumed that there will be no further changes required to the design, above and beyond those identified on the attached change schedules and as noted above. Given the exhaustive consultation approach that has been adopted with the relevant approvals bodies and the number of iterations of design that have been produced, comments received and subsequently incorporated, SDS believe the design to be robust and that all outstanding Technical and Prior Approvals could and should be granted by the relevant departments of the Approvals bodies without further change to the design. As such no allowance has been made for further design change in the production of the Guaranteed Maximum Price.

Specifically excluded from the Guaranteed Maximum Price are the following issues that at one point or another have been discussed, but not formalised or progressed by the Parties:

1. Stage E Landscape Drawings as per CEC Informative and Meeting held in September 2009;
2. South Gyle Access Bridge Detailed Design beyond Changes in S26 and W11 detailed in design as per DCR0133 feasibility report;
3. Tower Place Bridge design to reflect the removal of the ramp, concrete downstand, or revisions to the OLE, as per RDC154;
4. Forth Ports Section 1A fence alterations due to additional constraints in the Casino area;
5. Advanced Directional Signing for Constitution Street to address additional signs;
6. Lindsay Road phone mast construction conflicts;
7. Mounting of sign plates on buildings and railings;

8. Roseburn Street Viaduct curtain walls in lieu of the current fencing proposal at each abutment;
9. Murrayfield Tramstop Layout Changes to bollards, fence, and turnstiles;
10. Elder Street Junction redesign;
11. Picardy Place Detailed Design as per DCR0173 and DCR0221
12. Pedestrian Guardrail Changes;
13. Gogar Interchange name change to Edinburgh Gateway;
14. Relocation of the combined sewer at Gogar interchange.

Completion Dates and Liquidated Damages

Liquidated Damages will apply at the Infraco Contract rates and as per the terms of the Infraco Contract. However, the Planned Sectional Completion Dates are as identified in GMP Programme in Appendix 5 and are defined in the Executive Summary.

Liquidated Damages associated with tram delivery / tram commissioning in the Tram Supply Novation Agreement, the Tram Supply Agreement, and the Infraco Contract shall not apply any more, since the necessary progress of the tram delivery and tram commissioning has been included in the revised dates for the Planned Sectional Completion. The agreed commissioning dates for the Trams should be linked in the completion dates.

Schedule Part 4 Pricing Assumptions

We believe that a joint review of the existing Specified Exclusions and Pricing Assumptions in Schedule part 4 is required to enable agreement to be reached on a substantially reduced list.

Specific Contract Clause Modifications

We believe there is positive merit to the proposed Clause changes identified in your letter, and also believe that a joint review of the specific Clause modifications would enable both Parties to find agreement for the betterment of the Project.

Maintenance

The maintenance GMP is based on the current design as detailed in the GMP Drawings. If this is later subject to a Change Order, including any change to the agreed split of responsibilities within Schedule Part 2, then this shall be valued in accordance with clause 80.6.

The GMP is based on the assumption that the total maintenance period does not exceed 10 years starting with the service commencement date for Section D (as described in Appendix 5.2).

In view of the Project Carlisle revisions, we do not believe that certain conditions remain appropriate and we therefore we would like to review and to discuss the incorporation of the following points with respect to the Maintenance Services:

1. Removal of the voluntary termination by tie clause (89), which leads to a 10 year maintenance contract period.
2. Removal of the cap applicable to indexation on Spare Parts and infrastructure mobilization amounts, currently capped at a maximum aggregate amount of 50,000GBP.
3. Indexation: shall be for the full scope of maintenance, including mobilisation and initial spares.
4. Introduction of a Benchmarking point (at 5 years) regardless of whether there is any change in applicable law.
5. Renegotiation of Liability caps, as defined in Schedule Part 1, in the Maintenance Cap .

Maintenance Pricing

To reflect the Maintenance programming modifications required by Project Carlisle the Maintenance Pricing requires adjustment. The following Table identifies the Maintenance Pricing categories and Revised Amounts. Tram Maintenance and Mobilisation is included in the price provided by CAF, see APPENDIX 1.3 -CAF - Guaranteed Maximum Price Breakdown.

Maintenance Pricing

Infraco	Value
Infraco Maintenance limited to scope as defined in Infraco "Project Carlisle" Proposal	£25,133,013.00
Infraco Maintenance Mobilisation	£1,782,292.00
Infraco Spare Parts	£1,013,090.00
Total Infraco	£27,928,395.00
CAF	Value
Tram Maintenance	TBA
Maintenance Mobilization	£2,275,806.00
Total CAF	TBA

TBA = To Be Adjusted by CAF/Tie when Km/year of operation is decided (it is presumed that the total amount will be below the minimum foreseen in Tram Maintenance Agreement table for price adjustment according to distance and will therefore require an agreement of the price per kilometre. If that is not the case, TMA prices will be applied)

APPENDIX 5

INFRACO PROJECT CARLISLE PROGRAMME

5.0 Narrative

5.1 GMP Programme

5.2 Sectional Completion Dates

5.0 Narrative

General

The start date (data date) of the programme is 03 September 2010. For work activities currently under construction their actual progress at 03 September 2010 is taken as the start point of the programme.

Our programme for the works recognises the allowed working hours stated in the Code of Construction Practice of 0700 to 1900 Monday to Friday and 0800 to 1300 on a Saturday. However a 40 hour effective working week has been used to calculate the duration of activities to take account of rest breaks, un-exceptional weather delays and travel between work sites. However construction work will be required outside the hours stated in the Code of Construction Practice for Rail Possessions, work on busy road junctions, works associated with piling at the A8 underpass, stringing catenary cables etc. Prior approval will be sought for such works in accordance with the tender documents and the programme assumes that such approval will be granted as necessary to maintain the Programme. Assumptions and factual statements upon which the Programme has been prepared are as set out in this document. General assumptions are as follows.

Version 59 of the SDS design programme has been used to establish milestone dates for the Issue for Construction drawings. We have assumed that there will be no amendments to the SDS design programme.

The programme is based on MUDFA having completed all works and all utilities being diverted that would conflict with INFRACO operations.

The scope of work considered is that for and set out in this Proposal. Therefore, this programme as set out in Appendix 5 was set up only with regard to the Project Carlisle Proposal and based upon the scope provided herein.

Section Completion Dates

Section 1A

The programme for the enabling works at Newhaven which includes Lindsay Road Retaining Wall and the lowering of Lindsay Road is an extract of the relevant sections of the proposed Programme Revision 3A. The construction sequence therefore relies upon the availability of the Forth Ports diversion road. This diversion route permits the simultaneous construction of Lindsay Road Retaining Wall W1A and the lowering of Lindsay Road whilst still maintaining traffic flows.

The GMP Programme assumes a

The Programme assumes that all necessary Utility diversions have been completed in this area prior to the Infraco Works commencing.

The GMP Programme assumes that all 3rd Party approvals / licences will be in place for these works to commence in accordance on 01 November 2010.

The programme for Tower Place Bridge enabling works is extracted from the Programme Revision 3A and progressed to 03 September 2010. The programme only accounts for the tie relocation of utilities from North to South. Any delay in relocating the utilities, or additional works undertaken by tie may impact the sectional completion date.

Section 2A

The GMP programme assumes the termination point for the works to be the east end of Haymarket Viaduct. No track work is envisaged beyond the eastern end of the reinforced concrete viaduct deck.

The GMP Programme assumes a commencement date of 20 September 2010 for the removal and replacement of soft ground

Section 5A

Detailed programmes are available for all structures. The GMP Programme shows summaries of these programmes.

The construction works in general are subject to Network Rail approval. An eight week (plus three week "cooling off period") has been assumed for all Network Rail form C approvals. A four week (plus three week "cooling off period") has been allowed for the approval of all Works Package plans. The programme generally shows the latest dates for these approvals.

The GMP Programme assumes tie will procure all necessary approvals and issue the necessary Change Orders to allow the commencement of demolition in Plots 96, 97, 101 & 102, before 20 September 2010

Section 5C

The completion of the track and M & E works at the depot is dependent on the completion of the depot access bridge superstructure. The Programme assumes that the works to the depot access bridge will be accelerated to 7 days a week working from the 11 October 2010.

Section 6A

No comments.

Section 7A

The Programme assumes that the settlement period for the Gogar Landfill Site will be limited to 3 months and removal of the surcharge can commence on 15 October 2010.

The Programme also assumes that BAA will grant a licence for works on their land to recommence on 24 September 2010.

Revisions to the W14 Retaining Wall, to the extent that an alternative piling solution will be fully developed, have been included in the Programme. In the event that the approval process, by third parties, causes a delay beyond the dates included in the Programme, Infracore reserves the right to claim an extension of time.

Immediate Action Items

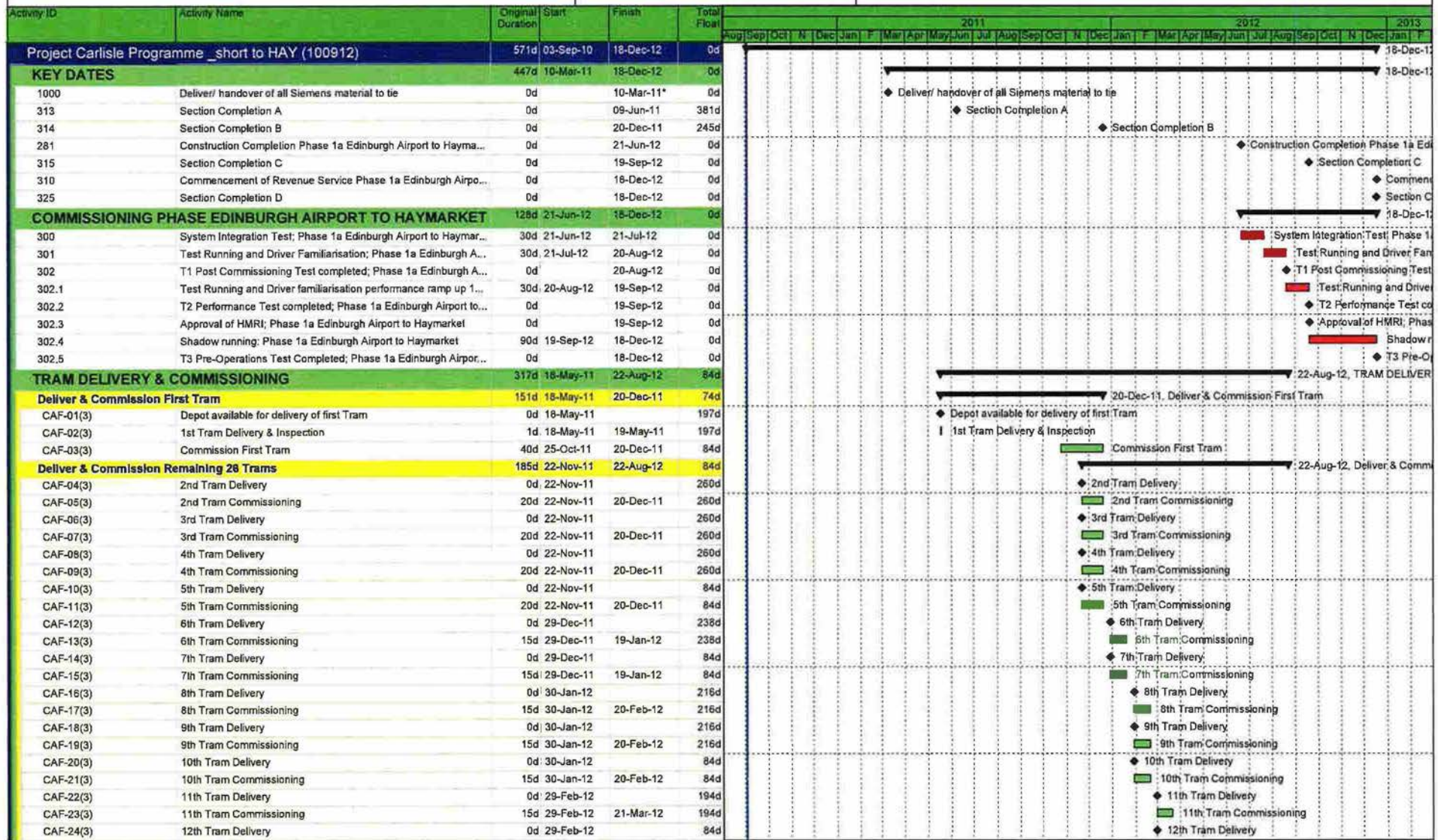
There are a number of time critical activities which require procurement of authorizations as well as procurement of long lead time materials. We propose to meet with tie to discuss this particular issue, as well as similar issues, within the next few days in an effort to find an agreeable solution to avoid unnecessary impacts to the Programme. The following list is not all inclusive:

1. Commencement of work at Lindsay Road in Section 1A by 01 November 2010
2. Removal and replacement of soft ground in Section 2A by 20 September 20120.
3. Permit to begin & Change Order issued for demolition in Section 5A by 20 September 2010.
4. Recommencement of work on BAA lands in Section 7A by 24 September 2010.
5. Finalisation of design to allow procurement of piling materials for Wall 14 C no later than 9 November 2010 to facilitate a 12 week fabrication and delivery requirement.

APPENDIX 5.1

INFRACO GMP PROGRAMME

Appendix 5.1 – GMP Programme:
Project Carlisle Programme – Airport to Haymarket
(Revision 01 100912)
Project Carlisle Programme – short to HAY (100912)
EDINBURGH TRAM NETWORK
Date 12-Sep-10
Pages 1 to 29



Date	Revision	Checked	Approved
12-Sep-10	Revision 01 (100912)	SCS/MHE	BSC

CEC00218042_0079

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2011												2012												2013											
						Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan	F					
						RW Structure						08-Dec-10, RW Structure						RW Foundation						RC Wall						Coping											
RW Finishing Works and Backfilling						04-Feb-11, RW Finishing Works and Backfilling						Waterproofing						Brickwork						Drainage																	
Lindsay Road Retaining Wall W1D						04-Oct-10, Lindsay Road Retaining Wall W1D						RW Structure						RC Wall						RW Finishing Works and Backfilling																	
Lindsay Road Retaining Wall W1C						05-Oct-10, Lindsay Road Retaining Wall W1C						RW Structure						Coping						RW Finishing Works and Backfilling																	
TTM PHASE 5B (LINDSAY ROAD WEST BOUND)						11-Apr-11, TTM PHASE 5B (LINDSAY ROAD WEST BOUND)						TTM - PHASE 5B - Lindsay Road EB closed						Commence TTM Works						TTM PHASE 5B COMPLETE																	
TEMPORARY AND PRELIMINARY WORKS						10-Nov-10, TEMPORARY AND PRELIMINARY WORKS						Establish temporary traffic management						Site clearance						Investigate services / outfall positions / inlet-outlet positions / levels																	
LINDSAY ROAD (WESTBOUND)						04-Apr-11, LINDSAY ROAD (WESTBOUND)																																			

Date	Revision	Checked	Approved
12-Sep-10	Revision 01 (100912)	SCS/MHE	BSC

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2011												2012					2013												
						Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan
1A-24-5B-1030	COMMENCE ROAD WORKS (LINDSAY ROAD-WESTBOUND)	0d	10-Nov-10		193d	◆ COMMENCE ROAD WORKS (LINDSAY ROAD-WESTBOUND)																													
1A-24-5B-1040	ROAD WORKS COMPLETE (LINDSAY ROAD)	0d		04-Apr-11	193d	◆ ROAD WORKS COMPLETE (LINDSAY ROAD)																													
WESTBOUND						◆ 29-Mar-11, WESTBOUND □ Remove existing footway □ Plane Main Carriageway & Footway □ Plane/break/lower / excavate to Formation (Lindsay Road) □ Capping layer (600mm) □ Drainage and Outfalls □ Ducts and Chambers □ Kerbing □ Footpath Works																													
1A-24-5B-1050	Remove existing footway	5d	10-Nov-10	17-Nov-10	193d																														
1A-24-5B-1060	Plane Main Carriageway & Footway	5d	17-Nov-10	24-Nov-10	193d																														
1A-24-5B-1070	Plane/break/lower / excavate to Formation (Lindsay Road)	49d	24-Nov-10	16-Feb-11	193d																														
1A-24-5B-1080	Capping layer (600mm)	49d	25-Nov-10	17-Feb-11	195d																														
1A-24-5B-1090	Drainage and Outfalls	49d	26-Nov-10	18-Feb-11	193d																														
1A-24-5B-1100	Ducts and Chambers	49d	26-Nov-10	18-Feb-11	193d																														
1A-24-5B-1110	Kerbing	10d	15-Feb-11	01-Mar-11	195d																														
1A-24-5B-1120	Footpath Works	27d	18-Feb-11	29-Mar-11	193d																														
SURFACING						▼ 07-Mar-11, SURFACING □ 220mm Sub-base □ 225mm Base □ 60mm Binder Course □ 35mm Surface Course - Main Carriageway and Boundary																													
1A-24-5B-1130	220mm Sub-base	1d	01-Mar-11	02-Mar-11	211d																														
1A-24-5B-1140	225mm Base	1d	02-Mar-11	03-Mar-11	211d																														
1A-24-5B-1150	60mm Binder Course	1d	03-Mar-11	04-Mar-11	211d																														
1A-24-5B-1160	35mm Surface Course - Main Carriageway and Boundary	1d	04-Mar-11	07-Mar-11	211d																														
ROAD FINISHING						▼ 04-Apr-11, ROAD FINISHING □ Road Signs and Markings □ Landscaping □ Remove temporary traffic management																													
1A-24-5B-1170	Road Signs and Markings	1d	07-Mar-11	08-Mar-11	211d																														
1A-24-5B-1180	Landscaping	9d	21-Mar-11	01-Apr-11	193d																														
1A-24-5B-1190	Remove temporary traffic management	1d	01-Apr-11	04-Apr-11	193d																														
COMMISSIONING AND TESTING						▼ 11-Apr-11, COMMISSIONING AND TESTING □ Handover / Acceptance of Roadworks and Trackworks ◆ Start remaining E&M works along track																													
1A-24-5B-TEST-100	Handover / Acceptance of Roadworks and Trackworks	5d	04-Apr-11	11-Apr-11	193d																														
1A-24-5B-TEST-110	Start remaining E&M works along track	0d	11-Apr-11		295d																														
TTM PHASE 5A (LINDSAY ROAD EAST BOUND)						◆ 06-Sep-11, TTM:PHASE 5A (LINDSAY ROAD EAST BOUND) ◆ TTM -PHASE 5A - Lindsay Road WB closed: ◆ TTM PHASE 5A COMPLETE ▼ 28-Apr-11, TEMPORARY AND PRELIMINARY WORKS □ Establish temporary traffic management: □ Site clearance □ Investigate services / outfall positions / inlet-outlet positions / levels ▼ 30-Jun-11, Lindsay Road Retaining Wall W1B ▼ 29-Apr-11, Preliminary works & excavation □ Mobilisation, site installation and temporary fencing □ Demolition □ Excavation ▼ 31-May-11, RW Structure □ RW Foundation □ RC Wall ▼ 30-Jun-11, RW Finishing Works and Backfilling □ Waterproofing □ Drainage □ Stone drainage trench □ Brickwork □ Backfill and compact □ Vehicle Parapets □ Kerbing □ Footway and Road Surfacing																													
1A-24-TTM-05A-1000	TTM - PHASE 5A - Lindsay Road WB closed	0d	11-Apr-11		193d																														
1A-24-TTM-05A-1010	TTM PHASE 5A COMPLETE	0d		06-Sep-11	193d																														
TEMPORARY AND PRELIMINARY WORKS																																			
1A-24-5A-1000	Establish temporary traffic management	1d	11-Apr-11	12-Apr-11	193d																														
1A-24-5A-1010	Site clearance	10d	12-Apr-11	27-Apr-11	193d																														
1A-24-5A-1020	Investigate services / outfall positions / inlet-outlet positions / le...	1d	27-Apr-11	28-Apr-11	193d																														
Lindsay Road Retaining Wall W1B																																			
Preliminary works & excavation																																			
1A-24-5A-W1B-100	Mobilisation, site installation and temporary fencing	1d	11-Apr-11	12-Apr-11	202d																														
1A-24-5A-W1B-110	Demolition	10d	12-Apr-11	27-Apr-11	202d																														
1A-24-5A-W1B-120	Excavation	2d	27-Apr-11	29-Apr-11	202d																														
RW Structure																																			
1A-24-5A-W1B-130	RW Foundation	9d	29-Apr-11	13-May-11	202d																														
1A-24-5A-W1B-140	RC Wall	11d	13-May-11	31-May-11	202d																														
RW Finishing Works and Backfilling																																			
1A-24-5A-W1B-150	Waterproofing	2d	31-May-11	02-Jun-11	202d																														
1A-24-5A-W1B-160	Drainage	5d	02-Jun-11	09-Jun-11	202d																														
1A-24-5A-W1B-170	Stone drainage trench	2d	09-Jun-11	13-Jun-11	202d																														
1A-24-5A-W1B-190	Brickwork	2d	09-Jun-11	13-Jun-11	202d																														
1A-24-5A-W1B-180	Backfill and compact	1d	13-Jun-11	14-Jun-11	202d																														
1A-24-5A-W1B-200	Vehicle Parapets	2d	14-Jun-11	16-Jun-11	212d																														
1A-24-5A-W1B-210	Kerbing	2d	14-Jun-11	16-Jun-11	202d																														
1A-24-5A-W1B-220	Footway and Road Surfacing	10d	16-Jun-11	30-Jun-11	202d																														

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■ Actual Work
 ■ Critical Remaining Work
 ▼ Summary
■ Remaining Work
 ◆ Milestone

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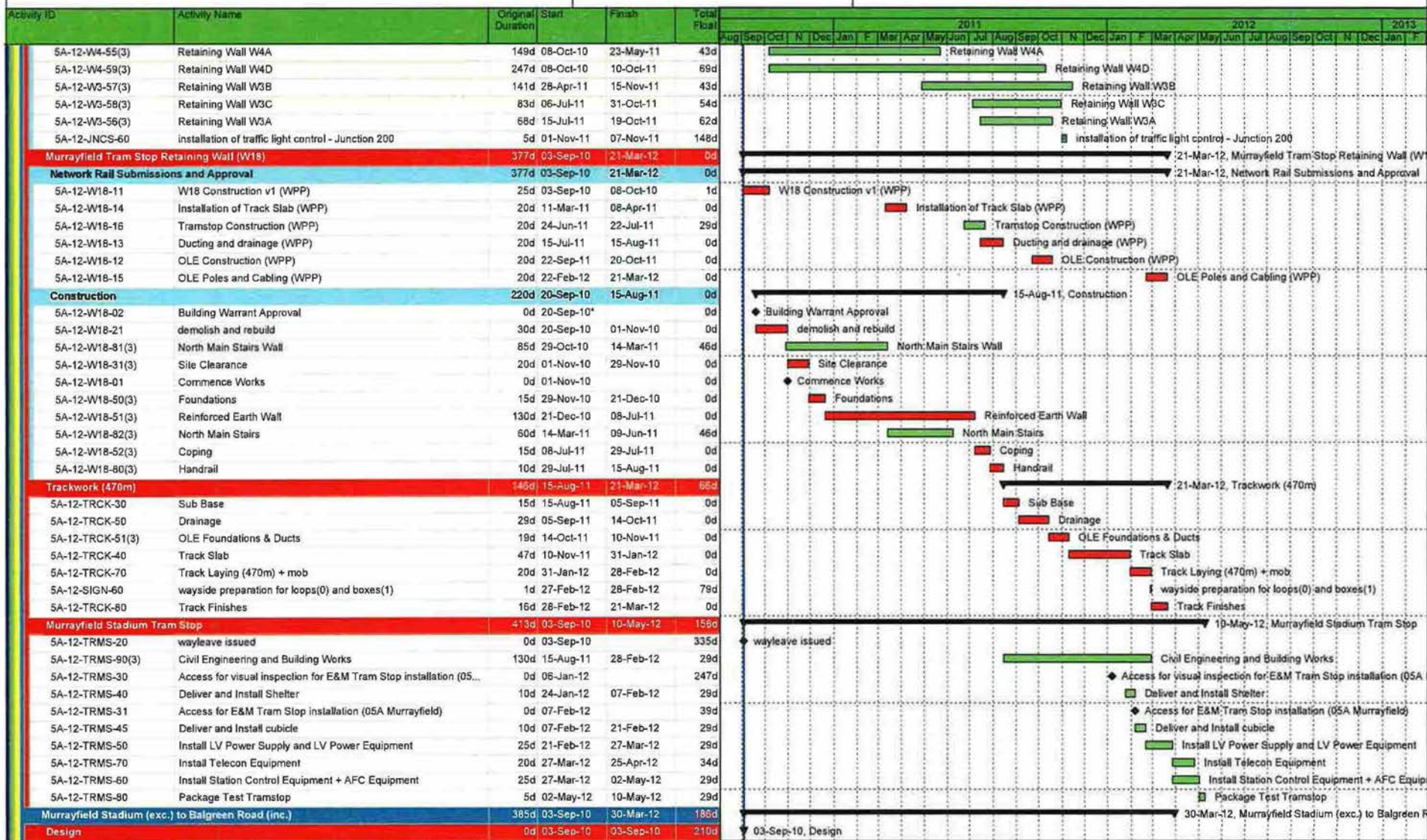
Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2011												2012												2013											
						Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan	F					
Concrete works to North Deck section						09-Feb-11, Concrete works to North Deck section:																																			
1A-1-S17-1700	Install formwork to deck slab extension	47d	22-Nov-10	09-Feb-11	315d	Install formwork to deck slab extension																																			
1A-1-S17-1710	Install reinforcement to deck slab	25d	22-Nov-10	10-Jan-11	313d	Install reinforcement to deck slab																																			
1A-1-S17-1720	Pour concrete to deck slab	16d	13-Dec-10	17-Jan-11	313d	Pour concrete to deck slab																																			
1A-1-S17-1730	Install formwork for deck cope	15d	15-Dec-10	18-Jan-11	313d	Install formwork for deck cope																																			
1A-1-S17-1740	Pour concrete to deck cope	14d	17-Dec-10	19-Jan-11	313d	Pour concrete to deck cope																																			
1A-1-S17-1770	Strip formwork to underside of deck section	23d	10-Jan-11	09-Feb-11	313d	Strip formwork to underside of deck section																																			
1A-1-S17-1750	Strip cope formwork	2d	20-Jan-11	21-Jan-11	313d	Strip cope formwork																																			
1A-1-S17-1780	Waterproofing to deck concrete	2d	24-Jan-11	25-Jan-11	313d	Waterproofing to deck concrete																																			
1A-1-S17-1780	Install pedestrian/cycleway barrier to cope	2d	26-Jan-11	27-Jan-11	321d	Install pedestrian/cycleway barrier to cope																																			
South access ramps						01-Mar-11, South access ramps:																																			
West ramp						14-Jan-11, West ramp																																			
1A-1-S17-1830	Excavate area for south West ramp	66d	30-Sep-10	14-Jan-11	486d	Excavate area for south West ramp																																			
1A-1-S17-1840	Blinding for precast units	2d	05-Oct-10	06-Oct-10	465d	Blinding for precast units																																			
1A-1-S17-1850	Place precast ramp units	2d	26-Nov-10	29-Nov-10	429d	Place precast ramp units																																			
1A-1-S17-1860	In situ ramp sections	12d	01-Dec-10	16-Dec-10	429d	In situ ramp sections																																			
1A-1-S17-1870	Bitumen paint to concrete	2d	17-Dec-10	20-Dec-10	429d	Bitumen paint to concrete																																			
1A-1-S17-1880	Backfill paving area	3d	21-Dec-10	23-Dec-10	429d	Backfill paving area																																			
1A-1-S17-1890	Install paving to footway	4d	24-Dec-10	12-Jan-11	429d	Install paving to footway																																			
1A-1-S17-1900	Install pedestrian/cycleway barrier	2d	13-Jan-11	14-Jan-11	429d	Install pedestrian/cycleway barrier																																			
1A-1-S17-1910	WEST RAMP COMPLETE	0d		14-Jan-11	481d	WEST RAMP COMPLETE																																			
East ramp						01-Mar-11, East ramp																																			
1A-1-S17-1920	Excavate area for South East ramp	3d	17-Jan-11	19-Jan-11	429d	Excavate area for South East ramp																																			
1A-1-S17-1930	Excavate area for retaining wall	2d	20-Jan-11	21-Jan-11	429d	Excavate area for retaining wall																																			
1A-1-S17-1940	Construct retaining wall	10d	24-Jan-11	04-Feb-11	429d	Construct retaining wall																																			
1A-1-S17-1950	Blinding for units	2d	07-Feb-11	08-Feb-11	429d	Blinding for units																																			
1A-1-S17-1960	Place precast ramp units	2d	09-Feb-11	10-Feb-11	429d	Place precast ramp units																																			
1A-1-S17-1970	Bitumen paint to concrete	2d	11-Feb-11	14-Feb-11	429d	Bitumen paint to concrete																																			
1A-1-S17-1980	Install trieff kerbs	3d	15-Feb-11	17-Feb-11	429d	Install trieff kerbs																																			
1A-1-S17-1990	Install Aco channel	2d	18-Feb-11	21-Feb-11	429d	Install Aco channel																																			
1A-1-S17-2000	Install deterrant paving	2d	22-Feb-11	23-Feb-11	429d	Install deterrant paving																																			
1A-1-S17-2010	Install pedestrian/cycleway barrier	2d	24-Feb-11	25-Feb-11	429d	Install pedestrian/cycleway barrier																																			
1A-1-S17-2020	Install paving to footway	2d	28-Feb-11	01-Mar-11	429d	Install paving to footway																																			
1A-1-S17-2030	EAST RAMP COMPLETE	0d		01-Mar-11	449d	EAST RAMP COMPLETE																																			
Pumping house retaining wall						29-Mar-11, Pumping house retaining wall																																			
1A-1-S17-2040	Works TBC	20d	02-Mar-11	29-Mar-11	429d	Works TBC																																			
1A-1-S17-2050	PUMPING HOUSE RW COMPLETE	0d		29-Mar-11	429d	PUMPING HOUSE RW COMPLETE																																			
Road & Track works						15-Mar-11, Road & Track works																																			
1A-1-1020	Plane/excavate existing road surface (Tower Bridge)	4d	17-Feb-11	22-Feb-11	313d	Plane/excavate existing road surface (Tower Bridge)																																			
1A-1-1030	Remove existing deck waterproofing	3d	23-Feb-11	25-Feb-11	313d	Remove existing deck waterproofing																																			
1A-1-1040	New waterproofing and sand carpet	3d	28-Feb-11	02-Mar-11	313d	New waterproofing and sand carpet																																			
1A-1-1060	Surfacing to carriageway	5d	03-Mar-11	09-Mar-11	313d	Surfacing to carriageway																																			
1A-1-1070	Road markings	4d	10-Mar-11	15-Mar-11	313d	Road markings																																			
SECTION 2 HAYMARKET CORRIDOR						11-Apr-12, SECTION 2 HAYMARKET CORRIDOR																																			
02A Haymarket Corridor						11-Apr-12, 02A Haymarket Corridor																																			
E & M Installations						11-Apr-12, E & M Installations																																			

Date	Revision	Checked	Approved
12-Sep-10	Revision 01 (100912)	SCS/MHE	BSC

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Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2011												2012												2013											
						Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan	F					
2A-13-OHLE-40	Access for visual inspection for E&M wayside installation HAY - ...	0d	28-Jun-11		402d	◆ Access for visual inspection for E&M wayside installation HAY - ROJ																																			
2A-13-ELEC-40	Access visual inspection to cable pits for cable laying HAY - ROJ	0d	28-Jun-11		402d	◆ Access visual inspection to cable pits for cable laying HAY - ROJ																																			
2A-13-OHLE-41	Access for E&M wayside installation HAY - ROJ	0d	28-Jul-11		239d	◆ Access for E&M wayside installation HAY - ROJ																																			
2A-13-ELEC-41	Access to cable pits for cable laying HAY - ROJ	0d	28-Jul-11		221d	◆ Access to cable pits for cable laying HAY - ROJ																																			
2A-13-SIGN-40	Installation of SIG-interlocking cubicle HAY Yards	15d	28-Jul-11	17-Aug-11	234d	■ Installation of SIG-interlocking cubicle HAY Yards																																			
2A-13/14-OHLE-50	Installation of poles and building fixings -11 (13 1234 - 20 0566)	4d	28-Jul-11	02-Aug-11	204d	■ Installation of poles and building fixings -11 (13 1234 - 20 0566)																																			
2A-13/14-OHLE-60	Overhead Catenary Line -11 (13 1234 - 20 0566 + 20 0249 - 20...	11d	03-Aug-11	14-Sep-11	204d	■ Overhead Catenary Line -11 (13 1234 - 20 0566 + 20 0249 - 20 0437)																																			
2A-13-SIGN-90	installation of SIG equipment on HAY Yards	20d	11-Aug-11	07-Sep-11	234d	■ installation of SIG equipment on HAY Yards																																			
2A-13-SIGN-80	Commissioning of SIG -interlocking cubicle HAY Yards	5d	08-Sep-11	14-Sep-11	234d	■ Commissioning of SIG -interlocking cubicle HAY Yards																																			
2A-12/13-ELEC-60	Install Electrical Cables HAY - MUS	14d	21-Mar-12	11-Apr-12	49d	■ Install Electrical Cables HAY - MUS																																			
2A-12/13/14-TELC-60	Install Fibre Optic Cables SHP - HAY - MUS	11d	21-Mar-12	05-Apr-12	52d	■ Install Fibre Optic Cables SHP - HAY - MUS																																			
2A-12/13-SGNW-60	Install Signalling Wayside Equipment HAY - MUS	14d	21-Mar-12	11-Apr-12	49d	■ Install Signalling Wayside Equipment HAY - MUS																																			
Haymarket (inc.) to Roseburn Junction (inc.)		307d	03-Sep-10	29-Nov-11	297d	29-Nov-11, Haymarket (inc.) to Roseburn Junction (inc.)																																			
Haymarket Station Viaduct (S19)		137d	03-Sep-10	29-Mar-11	238d	29-Mar-11, Haymarket Station Viaduct (S19)																																			
2A-13-S19-60	Superstructure	62d	03-Sep-10	29-Nov-10	183d	■ Superstructure																																			
2A-13-S19-80	Finishes	15d	01-Dec-10	21-Dec-10	183d	■ Finishes																																			
2A-13-S19-90(3)	Crew Relief Facility	60d	22-Dec-10	29-Mar-11	238d	■ Crew Relief Facility																																			
Trackwork (1135m)		243d	03-Sep-10	30-Aug-11	352d	30-Aug-11, Trackwork (1135m)																																			
Network Rail Submissions & Approval		205d	03-Sep-10	06-Jul-11	207d	06-Jul-11, Network Rail Submissions & Approval																																			
2A-13-TRCK-12	Ducting and drainage (WPP)	20d	03-Sep-10	30-Sep-10	166d	■ Ducting and drainage (WPP)																																			
2A-13-TRCK-10	OLE Foundation Construction v1 (WPP)	20d	24-Sep-10	21-Oct-10	166d	■ OLE Foundation Construction v1 (WPP)																																			
2A-13-TRCK-11	OLE Construction (WPP)	20d	24-Sep-10	21-Oct-10	166d	■ OLE Construction (WPP)																																			
2A-13-TRCK-13	Installation of Track Slab (WPP)	20d	14-Oct-10	10-Nov-10	166d	■ Installation of Track Slab (WPP)																																			
2A-13-TRCK-14	OLE Poles and Cabling (WPP)	20d	09-Jun-11	06-Jul-11	205d	■ OLE Poles and Cabling (WPP)																																			
Construction		232d	20-Sep-10	30-Aug-11	362d	30-Aug-11, Construction																																			
2A-13-TRCK-30	Bulk Excavation - Sub Base	18d	20-Sep-10	13-Oct-10	172d	■ Bulk Excavation - Sub Base																																			
2A-13-TRCK-50	Drainage	16d	22-Oct-10	11-Nov-10	166d	■ Drainage																																			
2A-13-TRCK-50(3)	OLE Foundations & Ducts	14d	12-Nov-10	02-Dec-10	166d	■ OLE Foundations & Ducts																																			
2A-13-TRCK-40	Concrete Track Slab	23d	03-Dec-10	18-Jan-11	166d	■ Concrete Track Slab																																			
2A-13A-TRCK-70	Track Laying 1	19d	10-Feb-11	08-Mar-11	166d	■ Track Laying 1																																			
2A-13-SIGN-70	point machine mechanical set up	30d	10-Feb-11	23-Mar-11	449d	■ point machine mechanical set up																																			
2A-13-SIGN-60	wayside preparation for loops(17) and boxes(12)	23d	21-Feb-11	23-Mar-11	449d	■ wayside preparation for loops(17) and boxes(12)																																			
2A-13B-TRCK-70	Track Laying 2	10d	09-Mar-11	22-Mar-11	166d	■ Track Laying 2																																			
2A-13C-TRCK-70	Track Laying 3	14d	18-Apr-11	09-May-11	166d	■ Track Laying 3																																			
2A-13-TRCK-80	Track Finishes	38d	10-May-11	01-Jul-11	166d	■ Track Finishes																																			
2A-13-TRCK-80(3)	Concrete Channel Drain	18d	04-Jul-11	27-Jul-11	166d	■ Concrete Channel Drain																																			
2A-13-TRCK-81	Fencing	23d	28-Jul-11	30-Aug-11	166d	■ Fencing																																			
Road Works		61d	10-Jan-11	05-Apr-11	317d	05-Apr-11, Road Works																																			
2A-13C-ROAD-80(3)	Haymarket Yards	62d	10-Jan-11	05-Apr-11	249d	■ Haymarket Yards																																			
2A-13B-JNCS-60	installation of traffic light control - Junction 90	7d	09-Mar-11	17-Mar-11	320d	■ installation of traffic light control - Junction 90																																			
2A-13B-JNCS-70	installation of traffic light control - Junction 91	7d	18-Mar-11	28-Mar-11	320d	■ installation of traffic light control - Junction 91																																			
Haymarket Terrace Substation		249d	03-Sep-10	08-Sep-11	322d	08-Sep-11, Haymarket Terrace Substation																																			
2A-13-SUBS-110	wayleave granted (substation area)	0d	03-Sep-10		286d	◆ wayleave granted (substation area)																																			
2A-13-SUBS-120	meter issue for power supply clarified	0d	03-Sep-10		306d	◆ meter issue for power supply clarified																																			
2A-13-SUBS-90	Civil Engineering and Building Works	65d	22-Dec-10	05-Apr-11	183d	■ Civil Engineering and Building Works																																			
2A-13-SUBS-30	Access for visual inspection for E&M Substation installation (02...	0d	07-Mar-11		196d	◆ Access for visual inspection for E&M Substation installation (02A Haymarket Terrace)																																			

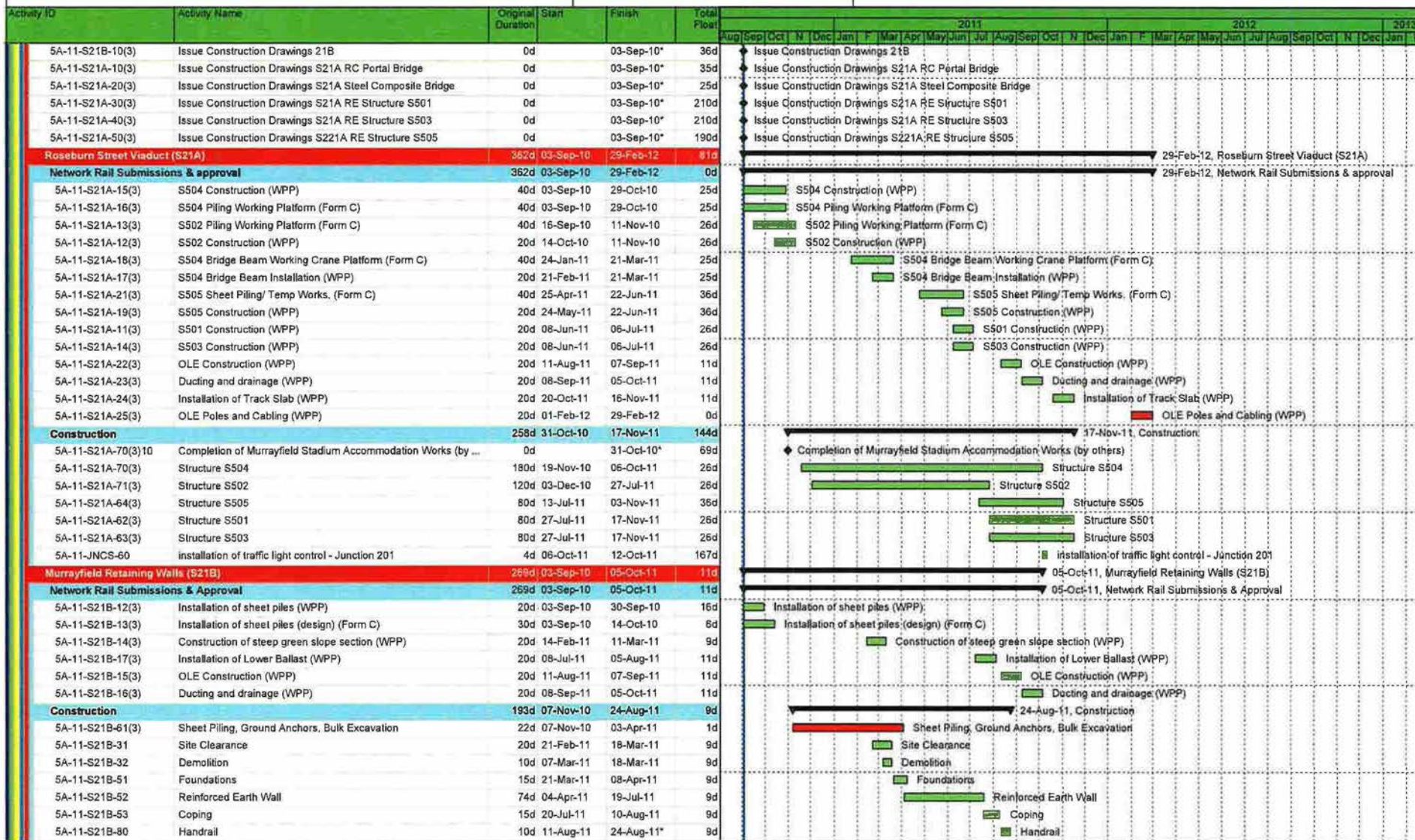
Date	Revision	Checked	Approved
12-Sep-10	Revision 01 (100912)	SCS/MHE	BSC



Date	Revision	Checked	Approved
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█ Actual Work █ Critical Remaining Work Summary
 Remaining Work ◆ Milestone

CEC00218042_0089



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12-Sep-10	Revision 01 (100912)	SCS/MHE	BSC

█ Actual Work █ Critical Remaining Work Summary
█ Remaining Work ◆ Milestone

CEC00218042_0090

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2011												2012												2013											
						Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan	F					
Murrayfield Stadium Underpass (S21C)						10-Aug-11, Murrayfield Stadium Underpass (S21C)																																			
Construction						10-Aug-11, Construction																																			
5A-11-S21C-31	Site Clearance	130d	04-Feb-11	10-Aug-11	9d	Site Clearance																																			
5A-11-S21C-32	Temporary Works - Demolition	20d	18-Feb-11	17-Mar-11	9d	Temporary Works - Demolition																																			
5A-11-S21C-32(3)	Additional Sheet Piling	20d	18-Mar-11	14-Apr-11	9d	Additional Sheet Piling																																			
5A-11-S21C-33	Earthworks	10d	15-Apr-11	29-Apr-11	9d	Earthworks																																			
5A-11-S21C-51	Base	10d	03-May-11	16-May-11	9d	Base																																			
5A-11-S21C-52	Walls	20d	17-May-11	14-Jun-11	9d	Walls																																			
5A-11-S21C-61	Roof	20d	15-Jun-11	12-Jul-11	9d	Roof																																			
5A-11-S21C-62	Parapet Walls	15d	06-Jul-11	26-Jul-11	9d	Parapet Walls																																			
5A-11-S21C-80	Finishes	10d	27-Jul-11	10-Aug-11	9d	Finishes																																			
Network Rail Submissions & Approval						24-Feb-11, Network Rail Submissions & Approval																																			
5A-11-S21C-25(3)	Network Rail Form C Submission & Approval	25d	03-Sep-10	07-Oct-10	68d	Network Rail Form C Submission & Approval																																			
5A-11-S21C-11(3)	Crane Working Platform (Form C)	25d	03-Sep-10	07-Oct-10	78d	Crane Working Platform (Form C)																																			
5A-11-S21C-13(3)	Soil Nailing Working Platform (Form C)	25d	03-Sep-10	07-Oct-10	98d	Soil Nailing Working Platform (Form C)																																			
5A-11-S21C-10(3)	Construction v2 (WPP)	20d	03-Dec-10	13-Jan-11	9d	Construction v2 (WPP)																																			
5A-11-S21C-12(3)	Soil Nailing v2 (WPP)	20d	28-Jan-11	24-Feb-11	9d	Soil Nailing v2 (WPP)																																			
Murrayfield Retaining Walls (S21D)						13-Feb-12, Murrayfield Retaining Walls (S21D)																																			
Network Rail Form C Submission & Approvals						13-Feb-12, Network Rail Form C Submission & Approvals																																			
5A-11-S21D-10(3)	Network Rail Form C West end Piling Platform Submission & Approval	40d	03-Sep-10*	28-Oct-10	68d	Network Rail Form C West end Piling Platform Submission & Approval																																			
5A-11-S21D-11(3)	Network Rail Form C West End Piling Submission & Approval	40d	03-Sep-10*	28-Oct-10	68d	Network Rail Form C West End Piling Submission & Approval																																			
5A-11-S21D-12(3)	Network Rail Form C toe Piling Platform Submission & Approval	40d	03-Sep-10*	28-Oct-10	68d	Network Rail Form C toe Piling Platform Submission & Approval																																			
5A-11-S21D-13(3)	Network Rail Form C Toe Sheet Piling Submission & Approval	40d	03-Sep-10*	28-Oct-10	68d	Network Rail Form C Toe Sheet Piling Submission & Approval																																			
5A-11-S21D-15(3)	Installation of sheet piling to western end (WoL) (WPP)	20d	03-Sep-10	30-Sep-10	63d	Installation of sheet piling to western end (WoL) (WPP)																																			
5A-11-S21D-14(3)	Construction of steep green slope section (WPP)	20d	23-Feb-11	22-Mar-11	28d	Construction of steep green slope section (WPP)																																			
5A-11-S21D-16(3)	Construction of mod block wall to western end (WoL) (WPP)	20d	01-Jun-11	28-Jun-11	341d	Construction of mod block wall to western end (WoL) (WPP)																																			
5A-11-S21D-13(3)10	Installation of Lower Ballast (WPP)	20d	08-Jul-11	05-Aug-11	11d	Installation of Lower Ballast (WPP)																																			
5A-11-S21D-17(3)	OLE Construction (WPP)	20d	11-Aug-11	07-Sep-11	11d	OLE Construction (WPP)																																			
5A-11-S21D-18(3)	Ducting and drainage (WPP)	20d	08-Sep-11	05-Oct-11	11d	Ducting and drainage (WPP)																																			
5A-11-S21D-13(3)20	OLE Poles and Cabling (WPP)	20d	17-Jan-12	13-Feb-12	15d	OLE Poles and Cabling (WPP)																																			
Construction						03-Aug-11, Construction																																			
5A-11-S21D-30(3)	Site Clearance / Testing / Design Check	13d	26-Nov-10	15-Dec-10	63d	Site Clearance / Testing / Design Check																																			
5A-11-S21D-61(3)	Sheet Piling	10d	16-Dec-10	12-Jan-11	63d	Sheet Piling																																			
5A-11-S21D-62(3)	Bulk Excavation	29d	13-Jan-11	12-Apr-11	28d	Bulk Excavation																																			
5A-11-S21D-62(3)10	Green Slope	67d	13-Apr-11	19-Jul-11	28d	Green Slope																																			
5A-11-S21D-80	Parapet	10d	20-Jul-11	03-Aug-11	28d	Parapet																																			
Water of Leith Bridge (S21E)						26-Aug-11, Water of Leith Bridge (S21E)																																			
Network Rail Form C Submission & Approval						11-Apr-11, Network Rail Form C Submission & Approval																																			
5A-11-S21E-13(3)	Mobile crane working platform to construct bridge (Form C)	5d	03-Sep-10	09-Sep-10	16d	Mobile crane working platform to construct bridge (Form C)																																			
5A-11-S21E-14(3)	Installation of sheet piling to centre pier (WPP)	5d	03-Sep-10	09-Sep-10	16d	Installation of sheet piling to centre pier (WPP)																																			
5A-11-S21E-15(3)	Installation of bored piling to centre pier (WPP)	5d	03-Sep-10	09-Sep-10	16d	Installation of bored piling to centre pier (WPP)																																			
5A-11-S21E-16(3)	Construction of centre pier (WPP)	5d	03-Sep-10	09-Sep-10	16d	Construction of centre pier (WPP)																																			
5A-11-S21E-11(3)	Sheet piling working platform to centre pier (Form C)	5d	03-Sep-10	09-Sep-10	16d	Sheet piling working platform to centre pier (Form C)																																			
5A-11-S21E-12(3)	Bored piling working platform to centre pier (Form C)	5d	03-Sep-10	09-Sep-10	16d	Bored piling working platform to centre pier (Form C)																																			
5A-11-S21E-17(3)	Construction of abutments (WPP)	5d	02-Dec-10	08-Dec-10	64d	Construction of abutments (WPP)																																			
5A-11-S21E-18(3)	Construction of deck (WPP)	20d	15-Mar-11	11-Apr-11	11d	Construction of deck (WPP)																																			

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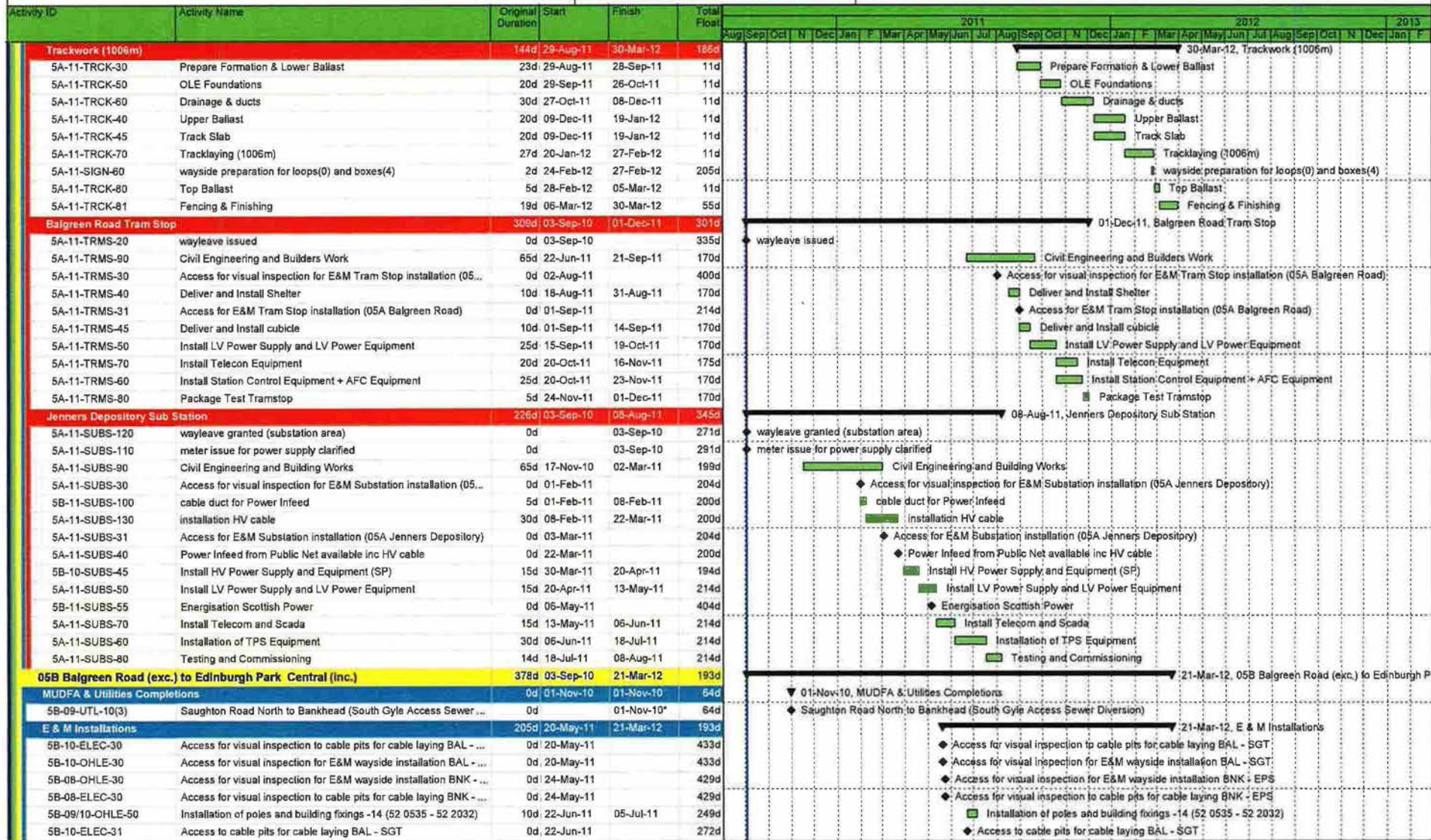


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■ Actual Work ■ Critical Remaining Work ▼ Summary
■ Remaining Work ◆ Milestone

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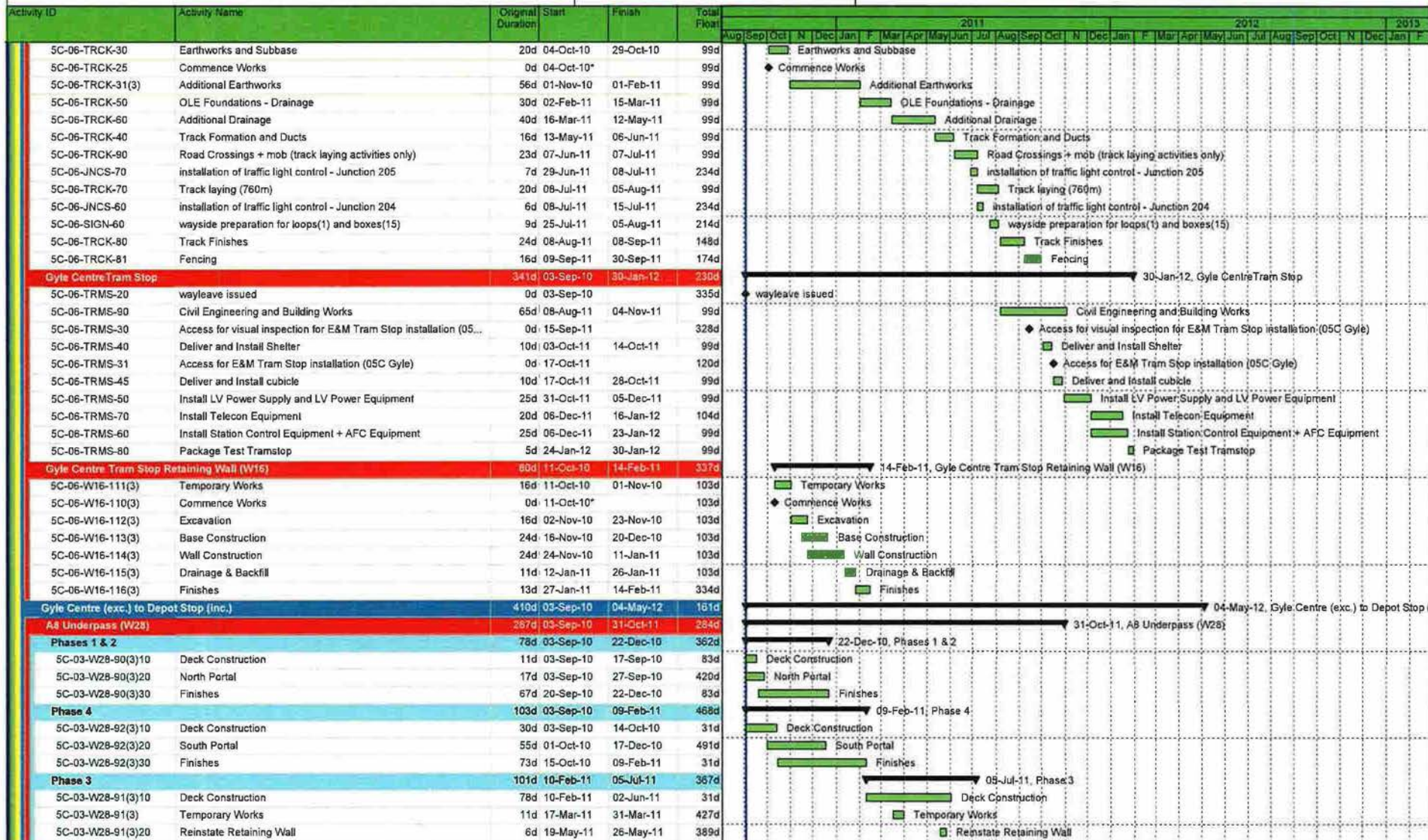
Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2011												2012					2013												
						Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan
Superstructure						28-Oct-10, Superstructure																													
Deck Finishes						28-Oct-10, Deck Finishes																													
5B-10-S23-82	Waterproofing (inc. deck curing)	3d	03-Sep-10*	08-Sep-10	452d	Waterproofing (inc. deck curing)																													
5B-10-S23-83	Walkway Construction	12d	03-Sep-10*	21-Sep-10	349d	Walkway Construction																													
5B-10-S23-80	Expansion Joints	2d	09-Sep-10	10-Sep-10	426d	Expansion Joints																													
5B-10-S23-81	Parapet Rails	5d	22-Oct-10	28-Oct-10	426d	Parapet Rails																													
Run On Run Off Slabs						22-Oct-10, Run On Run Off Slabs																													
5B-10-S23-71	Backfill and cast slab south	8d	04-Oct-10	13-Oct-10	399d	Backfill and cast slab south																													
5B-10-S23-70	Backfill and cast slab north	8d	13-Oct-10	22-Oct-10	396d	Backfill and cast slab north																													
Tracklaying						03-Nov-10, Tracklaying																													
5B-10B-S23-84	Containment Slabs	20d	03-Sep-10*	30-Sep-10	412d	Containment Slabs																													
5B-10-S23-85	Drainage	15d	22-Sep-10	12-Oct-10	412d	Drainage																													
5B-10B-TRCK-70	Fix Rails	8d	25-Oct-10	03-Nov-10	396d	Fix Rails																													
5B-10C-SIGN-600	wayside preparation for loops(0) and boxes(2)	1d	03-Nov-10	03-Nov-10	396d	wayside preparation for loops(0) and boxes(2)																													
Trackwork (1690m)						21-Jul-11, Trackwork (1690m)																													
Concrete Slab Track 590m						21-Jul-11, Concrete Slab Track 590m																													
5B-10C-TRCK-30	Earthworks- Subbase	34d	22-Sep-10	08-Nov-10	326d	Earthworks- Subbase																													
5B-10C-TRCK-40	OLE Foundations - Drainage - Ducts	24d	09-Nov-10	13-Dec-10	326d	OLE Foundations - Drainage - Ducts																													
5B-10C-TRCK-41(3)	Additional Drainage	21d	14-Dec-10	25-Jan-11	326d	Additional Drainage																													
5B-10C-TRCK-70	Track laying	10d	22-Jun-11	05-Jul-11	326d	Track laying																													
5B-10C-SIGN-60	wayside preparation for loops(0) and boxes(3)	2d	04-Jul-11	05-Jul-11	338d	wayside preparation for loops(0) and boxes(3)																													
5B-10C-TRCK-80	Fencing & Finishes	12d	06-Jul-11	21-Jul-11	326d	Fencing & Finishes																													
Ballasted Track 1160m						21-Jun-11, Ballasted Track 1160m																													
5B-10A-TRCK-30	Earthworks - Subbase	20d	03-Sep-10	30-Sep-10	170d	Earthworks - Subbase																													
5B-10A-TRCK-31(3)	Additional Earthworks	10d	01-Oct-10	14-Oct-10	170d	Additional Earthworks																													
5B-10A-TRCK-50	OLE Foundations - Drainage	23d	15-Oct-10	16-Nov-10	170d	OLE Foundations - Drainage																													
5B-10A-TRCK-60	Additional Drainage	20d	17-Nov-10	15-Dec-10	170d	Additional Drainage																													
5B-10A-TRCK-40	Bottom Ballast - Ducts	23d	16-Dec-10	31-Jan-11	170d	Bottom Ballast - Ducts																													
5B-10A-TRCK-70	Track laying (1160m)	21d	28-Mar-11	26-Apr-11	170d	Track laying (1160m)																													
5B-10A-SIGN-60	wayside preparation for loops(0) and boxes(5)	2d	25-Apr-11	26-Apr-11	323d	wayside preparation for loops(0) and boxes(5)																													
5B-10A-TRCK-76	Top Ballast	15d	27-Apr-11	18-May-11	170d	Top Ballast																													
5B-10A-TRCK-80	Fencing & Trackside Finishes	23d	19-May-11	21-Jun-11	170d	Fencing & Trackside Finishes																													
Saughton Road North Tram Stop						28-Feb-11, Saughton Road North Tram Stop																													
5B-10-TRMS-20	wayleave issued	0d	03-Sep-10		295d	wayleave issued																													
5B-10-TRMS-90	Civil Engineering and Building Works	65d	06-Sep-10*	06-Dec-10	284d	Civil Engineering and Building Works																													
5B-10-TRMS-30	Access for visual inspection for E&M Tram Stop installation (05B Saughton Road North)	0d	14-Oct-10		568d	Access for visual inspection for E&M Tram Stop installation (05B Saughton Road North)																													
5B-10-TRMS-40	Deliver and Install Shelter	10d	01-Nov-10	12-Nov-10	284d	Deliver and Install Shelter																													
5B-10-TRMS-31	Access for E&M Tram Stop installation (05B Saughton Road North)	0d	15-Nov-10		360d	Access for E&M Tram Stop installation (05B Saughton Road North)																													
5B-10-TRMS-45	Deliver and Install cubicle	10d	15-Nov-10	26-Nov-10	284d	Deliver and Install cubicle																													
5B-10-TRMS-50	Install LV Power Supply and LV Power Equipment	25d	29-Nov-10	17-Jan-11	324d	Install LV Power Supply and LV Power Equipment																													
5B-10-TRMS-70	Install Telecon Equipment	20d	18-Jan-11	14-Feb-11	329d	Install Telecon Equipment																													
5B-10-TRMS-60	Install Station Control Equipment + AFC Equipment	25d	18-Jan-11	21-Feb-11	324d	Install Station Control Equipment + AFC Equipment																													
5B-10-TRMS-80	Package Test Tramstop	5d	22-Feb-11	28-Feb-11	324d	Package Test Tramstop																													
Saughton Road North (exc.) to Bankhead (inc.)						20-Mar-12, Saughton Road North (exc.) to Bankhead																													
Bankhead Drive Substation						03-May-11, Bankhead Drive Substation																													
5B-09-SUBS-110	wayleave granted (substation area)	0d	03-Sep-10		278d	wayleave granted (substation area)																													
5B-09-SUBS-120	meter issue for power supply clarified	0d	03-Sep-10		291d	meter issue for power supply clarified																													

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Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2011												2012												2013											
						Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan	F	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	N	Dec	Jan	F					
5B-07-TRCK-31(3)	Additional Earthworks	57d	20-Sep-10	08-Dec-10	176d	Additional Earthworks																																			
5B-07-TRCK-50	OLE Foundations - Drainage	19d	09-Dec-10	18-Jan-11	176d	OLE Foundations - Drainage																																			
5B-07-TRCK-51(3)	Additional Drainage	17d	19-Jan-11	10-Feb-11	176d	Additional Drainage																																			
5B-07-TRCK-40	Track Formation - Ducts	16d	11-Feb-11	04-Mar-11	176d	Track Formation - Ducts																																			
5B-07-TRCK-70	Track laying (854m)	44d	10-May-11	11-Jul-11	176d	Track laying (854m)																																			
5B-07-SIGN-60	wayside preparation for loops(0) and boxes(4)	2d	08-Jul-11	11-Jul-11	402d	wayside preparation for loops(0) and boxes(4)																																			
5B-07-TRCK-80	Track Finishes	15d	12-Jul-11	02-Aug-11	228d	Track Finishes																																			
5B-07-TRCK-81	Fencing	10d	03-Aug-11	16-Aug-11	251d	Fencing																																			
Edinburgh Park Station Bridge (S27)		206d	03-Sep-10	11-Jul-11	185d	11-Jul-11, Edinburgh Park Station Bridge (S27)																																			
Network Rail Submissions and Approval		79d	18-Mar-11	11-Jul-11	185d	11-Jul-11, Network Rail Submissions and Approval																																			
5B-07-S27-10	Installation of Rails	20d	18-Mar-11	14-Apr-11	132d	Installation of Rails																																			
5B-07-S27-11	OLE Poles and Cabling	20d	14-Jun-11	11-Jul-11	184d	OLE Poles and Cabling																																			
Superstructure		30d	03-Sep-10	14-Oct-10	266d	14-Oct-10, Superstructure																																			
5B-07-S27-80	Bridge Finishes	30d	03-Sep-10	14-Oct-10	266d	Bridge Finishes																																			
05C Edinburgh Park Central (exc.) to Gogarburn (inc.)		410d	03-Sep-10	04-May-12	161d	04-May-12, D5C Edinburgh Park Central (exc.) to Gogarburn (inc.)																																			
MUDFA & Utilities Completions		0d	03-Sep-10	03-Sep-10	120d	03-Sep-10, MUDFA & Utilities Completions																																			
5C-06-UTL-10(3)	Edinburgh Park Central to Gyle	0d		03-Sep-10*	120d	Edinburgh Park Central to Gyle																																			
5C-03-UTL-10(3)	Gyle to Depot	0d		03-Sep-10*	120d	Gyle to Depot																																			
E & M Installations		285d	08-Mar-11	01-May-12	164d	01-May-12, E & M Installations																																			
5C-02-OHLE-40	Access for visual inspection for E&M wayside installation DEH - ...	0d	08-Mar-11		445d	Access for visual inspection for E&M wayside installation DEH - GBN																																			
5C-02-ELEC-40	Access for visual inspection to cable pits for cable laying DEH - ...	0d	08-Mar-11		445d	Access for visual inspection to cable pits for cable laying DEH - GBN																																			
5C-02-OHLE-41	Access for E&M wayside installation DEH - GBN	0d	07-Apr-11		255d	Access for E&M wayside installation DEH - GBN																																			
5C-02-ELEC-70	Install Electrical Cables DEH - GBN	6d	07-Apr-11	14-Apr-11	282d	Install Electrical Cables DEH - GBN																																			
5C-02-ELEC-41	Access to cable pits for cable laying DEH - GBN	0d	07-Apr-11		282d	Access to cable pits for cable laying DEH - GBN																																			
5C-06-ELEC-30	Access for visual inspection to cable pits for cable laying EDP - ...	0d	10-Aug-11		338d	Access for visual inspection to cable pits for cable laying EDP - GYL																																			
5C-06-OHLE-30	Access for visual inspection for E&M wayside installation EDP - ...	0d	10-Aug-11		338d	Access for visual inspection for E&M wayside installation EDP - GYL																																			
5C-06-ELEC-31	Access to cable pits for cable laying EDP - GYL	0d	09-Sep-11		167d	Access to cable pits for cable laying EDP - GYL																																			
5C-06/07-TELC-60	Install Fibre Optic Cables EPS - EDP - GYL	8d	09-Sep-11*	20-Sep-11	182d	Install Fibre Optic Cables EPS - EDP - GYL																																			
5C-06-SGNW-60	Install Signalling Wayside Equipment EDP - GYL	10d	09-Sep-11	22-Sep-11	180d	Install Signalling Wayside Equipment EDP - GYL																																			
5C-06/07-OHLE-50	Installation of poles and building fixings -17 (52 4265 - 53 0590)	7d	09-Sep-11*	19-Sep-11	167d	Installation of poles and building fixings -17 (52 4265 - 53 0590)																																			
5C-06-OHLE-31	Access for E&M wayside installation EDP - GYL	0d	09-Sep-11		148d	Access for E&M wayside installation EDP - GYL																																			
5C-06-ELEC-60	Install Electrical Cables EDP - GYL	10d	09-Sep-11	22-Sep-11	180d	Install Electrical Cables EDP - GYL																																			
5C-06/07-OHLE-60	Overhead Catenary Line -17 (52 4265 - 53 0590)	16d	20-Sep-11	11-Oct-11	167d	Overhead Catenary Line -17 (52 4265 - 53 0590)																																			
5C-03-ELEC-30	Access for visual inspection to cable pits for cable laying GYL - ...	0d	01-Feb-12		224d	Access for visual inspection to cable pits for cable laying GYL - ...																																			
5C-03-OHLE-30	Access for visual inspection for E&M wayside installation GYL - ...	0d	01-Feb-12		224d	Access for visual inspection for E&M wayside installation GYL - ...																																			
5C-01/03/06-TELC-60	Install Fibre Optic Cables EDP - GYL - DEH + DEP	15d	02-Mar-12	22-Mar-12	53d	Install Fibre Optic Cables EDP - GYL - DEH + DEP																																			
5C-03-SGNW-60	Install Signalling Wayside Equipment GYL - DEH	9d	02-Mar-12	14-Mar-12	61d	Install Signalling Wayside Equipment GYL - DEH																																			
5C-02/03/06-OHLE-50	Installation of poles and building fixings -18 (53 0456 - 53 1749)	12d	02-Mar-12	19-Mar-12	34d	Installation of poles and building fixings -18 (53 0456 - 53 1749)																																			
5C-03-ELEC-31	Access to cable pits for cable laying GYL - DEH	0d	02-Mar-12		53d	Access to cable pits for cable laying GYL - DEH																																			
5C-03-OHLE-31	Access for E&M wayside installation GYL - DEH	0d	02-Mar-12		34d	Access for E&M wayside installation GYL - DEH																																			
5C-03-ELEC-60	Install Electrical Cables GYL - DEH	9d	02-Mar-12	14-Mar-12	67d	Install Electrical Cables GYL - DEH																																			
5C-02-SGNW-70	Install Signalling Wayside Equipment DEH - GBN	6d	15-Mar-12	22-Mar-12	61d	Install Signalling Wayside Equipment DEH - GBN																																			
5C-02/03/06-OHLE-60	Overhead Catenary Line -18 (53 0456 - 53 1749)	30d	20-Mar-12	01-May-12	34d	Overhead Catenary Line -18 (53 0456 - 53 1749)																																			
5C-02/03-TELC-60	Install Fibre Optic Cables GYL - DEH - GBN	8d	23-Mar-12	03-Apr-12	53d	Install Fibre Optic Cables GYL - DEH - GBN																																			
Edinburgh Park (exc.) to Gyle Centre (inc.)		341d	03-Sep-10	30-Jan-12	230d	30-Jan-12, Edinburgh Park (exc.) to Gyle Centre (inc.)																																			
Trackwork (780m)		246d	04-Oct-10	30-Sep-11	183d	30-Sep-11, Trackwork (780m)																																			

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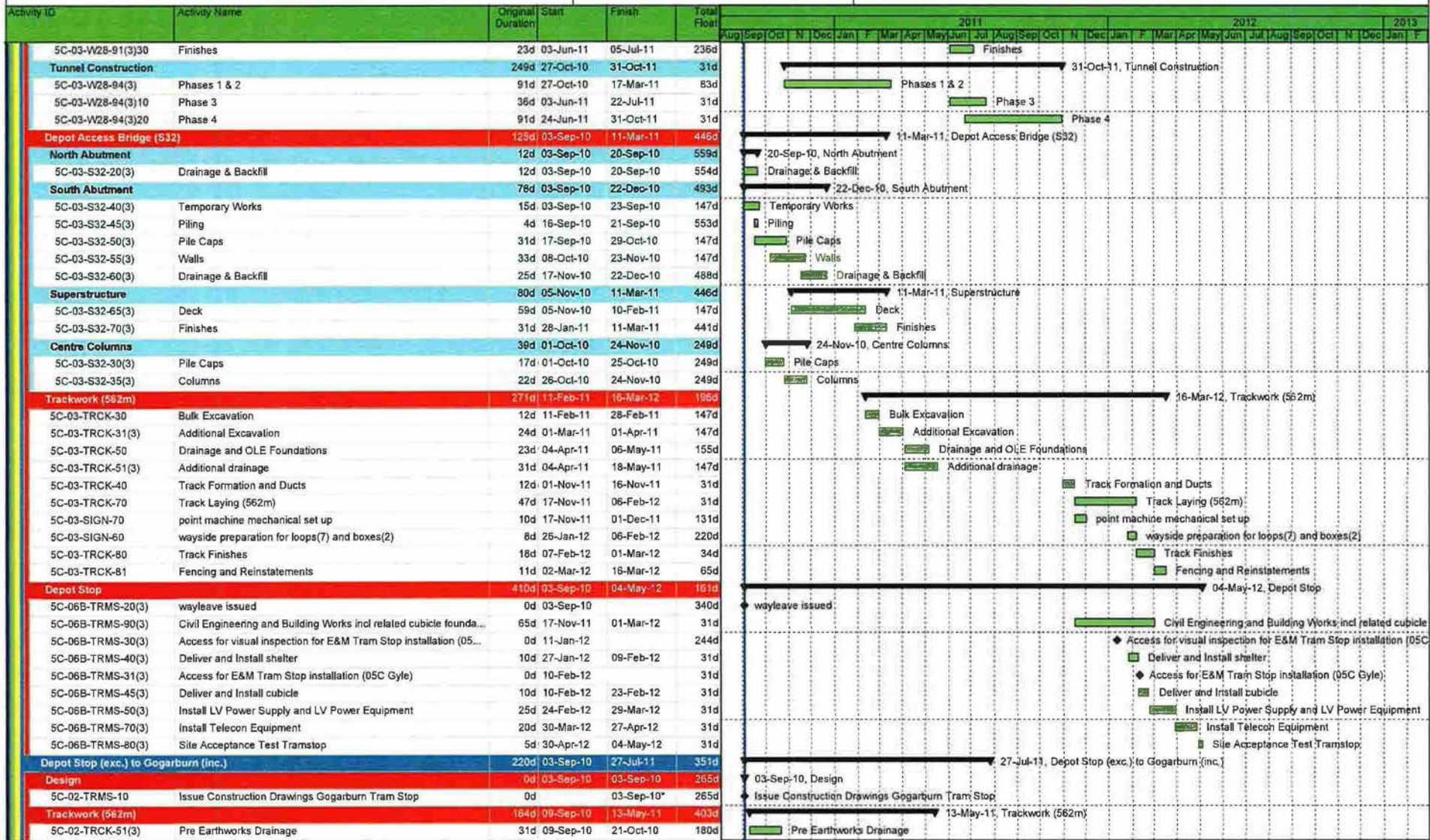
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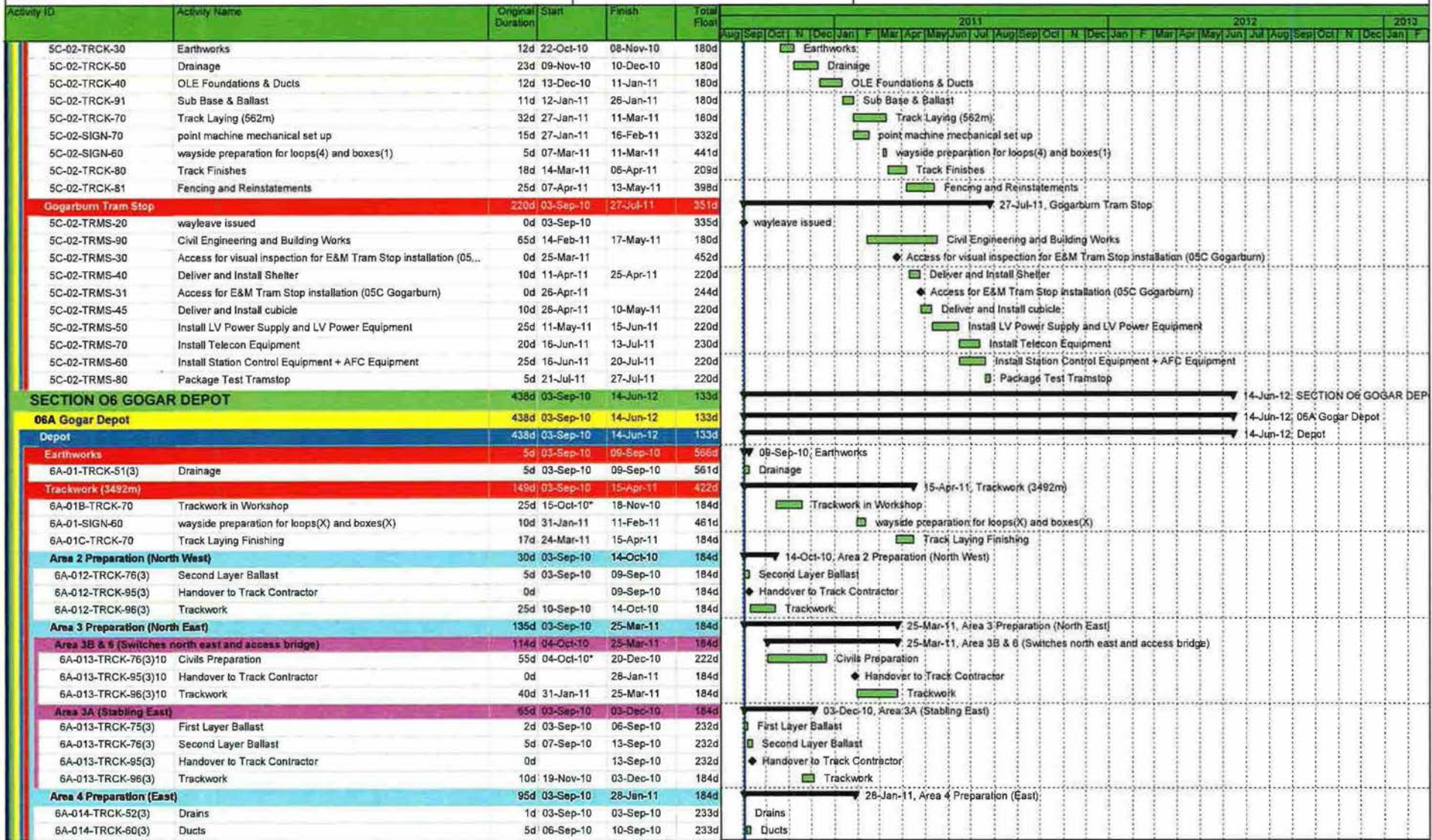
Project Carlisle - Airport to Haymarket

(Revision 01 100912)

**EDINBURGH
TRAM NETWORK**

BILFINGER BERGER SIEMENS CAF
Civil

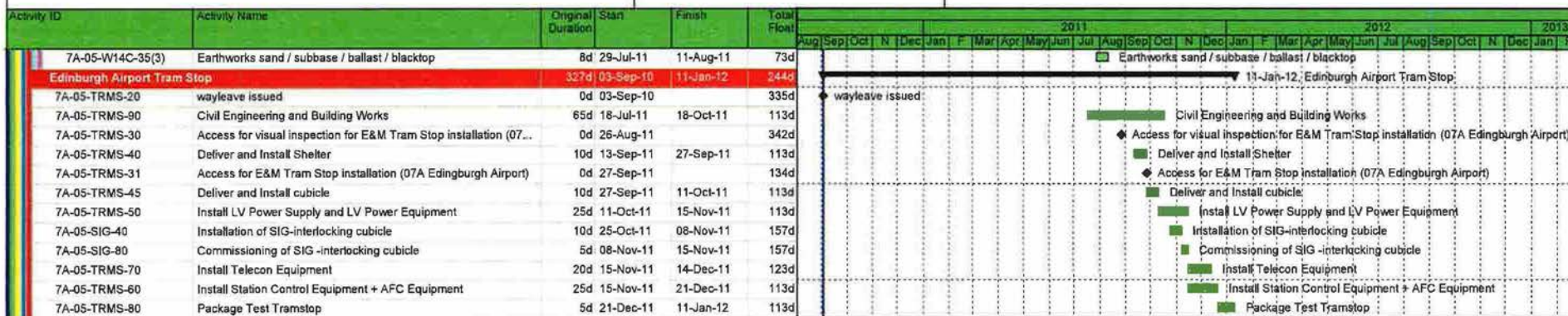
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Actual Work (Green bar) **Critical Remaining Work** (Red bar) **Summary** (Arrow)
Remaining Work (Light Green bar) **Milestone** (Diamond)

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APPENDIX 5.2

SECTIONAL COMPLETION DATES

Section	Scope	Target Date	Remarks
A	Section A means completion of the Depot (fully functional including energisation) and the first Tram delivered to the Site and assembled;	9 June 2011	finish of access bridge construction not mandatory for that milestone
B	Section B means completion of the test track functional from the Depot west entry/exit to the airport, and five Trams delivered to the Site and assembled with the first Tram to have completed the Tram Type Test and the remaining Trams to have completed the Tram Commissioning Routine Tests, such tests as described in the Employer's Requirements and the completion of all tests required to enable the commencement of Driver Training;	20 December 2011	Allows one year + shadow running remaining time for driver training - lineside systems fully functional - OCC and radio communication functional only as far as available
C	Section C means the carrying out and completion of the section from Airport to Haymarket and the completion of all tests required by the Employer's Requirements in relation to that Section, including those System Acceptance Tests that must be successfully completed prior to shadow running as provided for in the Employer's Requirements;	End of construction 21 June 2012 End of T&C 19 September 2012	3 month for Testing & Commissioning (OCC and radio communication fully functional)
D	Section D means the completion of shadow running and commencement of revenue service approval obtained and the completion of all tests required by the Employer's Requirements in relation to that Section, including those System Acceptance Tests that must be successfully completed to enable Service Commencement; Tram Commissioning Routine Tests performed for trams	18 December 2012	